

MENDOCINO COUNTY PLANNING COMMISSION

MINUTES

MAY 20, 2004

LOCATION: Mendocino County Board of Supervisors
Chambers, 501 Low Gap Road, Ukiah,
California

COMMISSIONERS PRESENT: McCowen, Barth, Nelson, Little, Edwards

COMMISSIONERS ABSENT: Lipmanson, Calvert

PLANNING & BLDG SVC STAFF PRESENT: Sandi Butterfield, Staff Assistant IV
Debra White, Office Manager
Raymond Hall, Director
Frank Lynch, Chief Planner

OTHER COUNTY DEPARTMENTS PRESENT: Frank Zotter, Deputy County Counsel

1. Roll Call.

The meeting was called to order at 9:03 a.m. Commissioners Lipmanson and Calvert were absent by pre-arrangement. Commissioner Nelson arrived at 9:23 a.m.

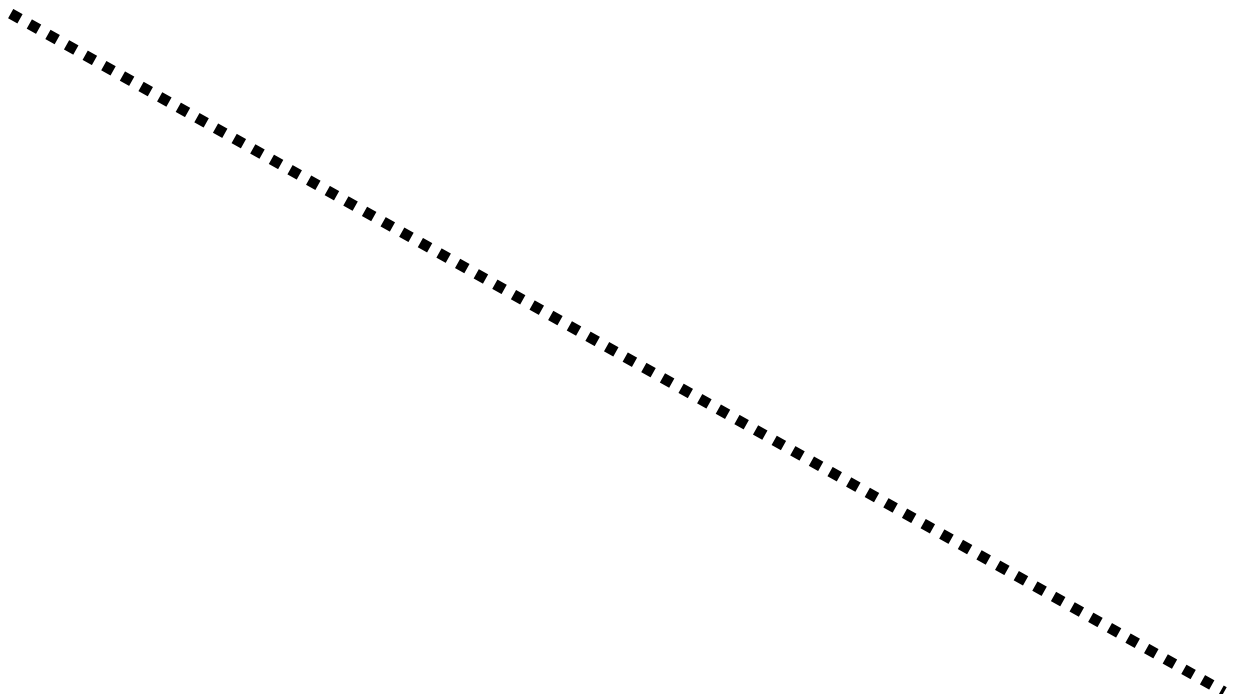
2. Determination of Legal Notice.

Mr. Lynch advised the Commission that all items have been properly noticed.

3. Director's Report and Miscellaneous.

Mr. Hall reviewed the written Director's Report submitted into the record.

4. Regular Calendar.



4D. SP 1-2003 – BROOKTRAILS SPECIFIC PLAN

Request: Amend the Brooktrails Specific Plan as proposed in Brooktrails Township Community Service District Board Resolution No. 2004-1 reflecting proposed changes to the adopted Specific Plan a result of the initial 5 year plan update recently completed by the BTCSD. Proposed amendments to Chapter 1-3, 7-8, 10-11 of the Brooktrails Specific Plan provide clarification to existing text and addresses topics including on-street parking, community circulation (second & third access, U.S. 101 Bypass), fire protection, water supply, and development standards.

Chairman McCowen noted that the Commission received a document from Arthur Eck regarding the proposal.

Mr. Pedroni reviewed the staff report.

Mr. Mike Chapman, General Manager of the Brooktrails Township Community Services District, summarized the history and adoption of the Specific Plan. He requested several corrections be made to the proposed amendment, as follows:

- Figure 7-1, on Page 7-3, should be corrected to show that Primrose Drive does not cross Sherwood Road and continue on to the east.
- Fifth paragraph on Page 7-4 – “... Route #3 is selected as the third preferred ~~second~~ access route largely because... and would provide a good third ~~second~~ evacuation route for residents...”
- Second paragraph on Page 7-5 – delete the last seven words in the paragraph; “~~and the new Southern Second Access Route.~~”
- Amend Paragraph B-1 on Page 10-31a – “All driveways and parking spaces, ~~except for single family dwellings with driveway slopes 10% or less,~~ are required to be paved...”

Mr. Chapman responded to points raised in the letter from the California Department of Transportation (Caltrans). Mr. Chapman felt that the issue raised in the first bullet on the Caltrans letter had been addressed on Page 1-2.

In response to Commissioner Barth, Mr. Chapman provided an update on the current status of the Willits bypass.

The public hearing was declared open.

Mr. Arthur Eck stated that most of the roads in Brooktrails are 24 feet wide or less and that any on street parking forces people to drive in the middle of the road. He stated that, if cars are parked on each side of the road, there is insufficient room to pass. Mr. Eck recommended that parking be restricted to provide for traffic safety, emergency vehicle access and to prevent erosion, which may occur when people park on unimproved shoulders. He suggested that parking on one side could be safely accommodated on roads that are 32 feet in width provided there is, at a minimum, 200 feet visibility in either direction. Parking on shoulders should be prohibited unless there is adequate surfacing of the shoulder.

Ms. Virginia Pohlson stated that she was pleased with the inclusion of the new language on Page 7-2. She requested clarification regarding required width of fire access roads and pointed out that the currently preferred second access route was not depicted on the map in Figure 7-1. Ms Pohlson described an additional potential emergency access that is not shown on Figure 7-1. She urged that the figure be updated.

Mr. Chapman commented that he would be providing an updated map to the Planning Department within the next week.

In response to Vice-chairman Little, Ms. Pohlson discussed the evacuation plan and commented that the plan needs to be updated.

The public hearing was declared closed.

Mr. Chapman stated that the Specific Plan Advisory Committee is working to identify suitable lots to serve as "parking pods" which could accommodate public parking. He noted that the roads within the township average 25 ½ feet wide. He advised that there are 60 miles of roads with 4 miles designated as fire lanes, within the township. In response to Chairman McCowen, Mr. Chapman clarified that the fire lanes are only available for emergency use and do not include any of the paved roads.

Vice-chairman Little expressed concern about approving the application without the map first being corrected.

Chairman McCowen suggested that the Commission take action on the application requiring that an accurate map is provided to and accepted by staff.

Commissioner Barth stated that she would prefer to have an updated letter from Caltrans regarding the bypass. Commissioner Barth supported having the District address on-street parking standards.

Director Hall discussed the status of the bypass and noted that Caltrans frequently changes the date for construction of the bypass. Current estimates state that construction will begin in 2007. He recommended deletion of any specific dates from the Plan regarding construction of the bypass.

Upon motion by Commissioner Little, seconded by Commissioner Nelson and carried by the following roll call vote, IT IS ORDERED that the Planning Commission recommends to the Board of Supervisors approval of the Brooktrails Specific Plan Amendment #SP 1-2003, making

the following findings, amending sections of the Specific Plan as detailed within Exhibit 2 of the May 20, 2004 staff report, incorporating the following additional amendments as requested by the General Manager of the Brooktrails Township Community Services District, and further requiring that Figure 7-1 be amended and submitted to staff prior to the Board of Supervisors hearing and further the Planning Commission recommends that the evacuation plan be updated and brought back to the Planning Commission and Board of Supervisors as an informational item:

1. **Environmental Findings:** That an initial study has been prepared and a Negative Declaration of environmental impacts is recommended for adoption finding that no significant environmental impacts are anticipated;
2. **General Plan Findings:** The project is consistent with the Mendocino County General Plan and the Brooktrails Specific Plan; and
3. **Department of Fish and Game Findings:** An Initial Study has been prepared evaluating the potential for adverse impacts and, when considering the record as a whole, there is no evidence in the record that the proposal will have potential for an adverse effect on wildlife resource or the habitat upon which wildlife depends. Therefore, the Commission rebuts the presumption set forth in subdivision (d) of Section 735.5.

Specific Plan Amendments: Amendments contained within Exhibit 2 of the May 20, 2004 staff report and the following additional amendments:

- Figure 7-1, on Page 7-3, should be corrected to show that Primrose Drive does not cross Sherwood Road and continue on to the east.
- Fifth paragraph on Page 7-4 – “... Route #3 is selected as the third preferred ~~second~~ access route largely because... and would provide a good third ~~second~~ evacuation route for residents...”
- Second paragraph on Page 7-5 – delete the last seven words in the paragraph; “~~and the new Southern Second Access Route.~~”
- Amend Paragraph B-1 on Page 10-31a – “All driveways and parking spaces, ~~except for single family dwellings with driveway slopes 10% or less,~~ are required to be paved...”

Discussion on Motion:

Commissioner Edwards recused himself from action on the application, explaining that he had done some work on the proposed second access and once discussion moved from the general concept of a second access to a specific location, he decided that it might not be appropriate for him to vote on the application. He noted he had not participated in the discussion and he then left the room.

Mr. Pedroni noted that the Department of Fish and Game commented that the de minimis finding was not appropriate for this application.

Commissioner Little stated that no new development would take place as a result of this application and he believed that the de minimis finding was appropriate. Other Commissioners concurred with Commissioner Little's comments.

AYES: Barth, Nelson, Little, McCowen

NOES: None

ABSENT: Calvert, Lipmanson, Edwards (Edwards recused himself and left the room)

5. Matters from Staff.

The Commission concurred with staff's recommendation to cancel the July 1, 2004 meeting.

6. Matters from Commission.

In response to Chairman McCowen, Mr. Lynch noted that the Board of Supervisors did approve the new fee schedule for the Department of Planning and Building Services.

7. Approval of Minutes.

None.

8. Matters from Public.

No one was present from the public to address the Commission.

9. Adjournment.

Upon motion by Commissioner Edwards, seconded by Commissioner Little, and unanimously carried (4-0; Calvert, Edwards and Lipmanson absent), IT IS ORDERED that the Planning Commission hearing adjourned at 11:37 a.m.



COUNTY OF MENDOCINO
DEPARTMENT OF PLANNING AND BUILDING SERVICES
501 LOW GAP ROAD • ROOM 1440 • UKIAH • CALIFORNIA • 95482

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April 23, 2004

NOTICE OF PUBLIC HEARING AND AVAILABILITY OF DRAFT NEGATIVE
DECLARATION FOR PUBLIC REVIEW

NOTICE IS HEREBY GIVEN THAT the Mendocino County Planning Commission at its regular meeting on Thursday, May 20, 2004, at 9:00 a.m., to be held in the Board of Supervisors Chambers, 501 Low Gap Road, Ukiah, California, will conduct a public hearing on the following project and the Draft Negative Declaration at the time listed or as soon thereafter as the item may be heard. As a matter of process, the item will also be heard by the Board of Supervisors at its Tuesday, June 22, 2004, meeting to be held in the Mendocino County Board of Supervisors Chambers, at 501 Low Gap Road, Ukiah, California at 1:30 p.m. or as soon thereafter as the item may be heard.

CASE#: SP 1-2003

OWNER: Numerous Property Owners within the Brooktrails Township

APPLICANT: Brooktrails Township Community Services District (BTCSD)

AGENT: Mike Chapman, General Manager

REQUEST: Amend the Brooktrails Specific Plan as proposed in Brooktrails Township Community Service District Board Resolution No. 2004-1 reflecting proposed changes to the adopted Specific Plan a result of the initial 5 year plan update recently completed by the BTCSD. Proposed amendments to Chapter 1-3, 7-8, 10-11 of the Brooktrails Specific Plan provide clarification to existing text and addresses topics including on-street parking, community circulation (second & third access, U.S. 101 Bypass), fire protection, water supply, and development standards.

LOCATION: Brooktrails Township located northwest of the City of Willits.

PROJECT COORDINATOR: Gary Pedroni

ENVIRONMENTAL DETERMINATION: The Department of Planning and Building Services has prepared a Draft Negative Declaration for the above project (no significant environmental impacts are anticipated). A copy of the Draft Negative Declaration is available for public review at 501 Low Gap Road, Room 1440, Ukiah, California and at 790 South Franklin Street, Fort Bragg, California. The Planning Commission staff report and notice are available on the Department of Planning and Building Services website at www.co.mendocino.ca.us/planning.

Your comments regarding the above project and/or the Draft Negative Declaration are invited. Written comments should be submitted to the Department of Planning and Building Services, at 501 Low Gap Road, Room 1440, Ukiah, California, 95482, no later than May 19, 2004. Oral comments may be presented to the Planning Commission/Board of Supervisors during the public hearing(s).

The Planning Commission's action regarding this item shall be a recommendation to the Board of Supervisors and the Board of Supervisors action shall be final. If you challenge the project in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Department of Planning and Building Services or the Planning Commission/Board of Supervisors at, or prior to, the public hearing(s). All persons are invited to appear and present testimony in this matter.

Additional information regarding the above noted item may be obtained by calling the Department of Planning and Building Services at 463-4281, Monday through Friday, 8:00 a.m. through 5:00 p.m. Should you desire notification of the Planning Commission/Board of Supervisor's decision you may do so by requesting notification in writing and providing a self-addressed stamped envelope to the Department of Planning and Building Services.

RAYMOND HALL, Secretary to the Planning Commission

OWNER: Property owners within the Brooktrails Township

APPLICANT: Brooktrails Township Community Services District (BTCSD)
24860 Birch Street
Willits CA 95490

AGENT: General Manager, Mr. Mike Chapman

REQUEST: Amend the Brooktrails Specific Plan as proposed in Brooktrails Township Community Service District Board Resolution No. 2004-1 reflecting proposed changes to the adopted Specific Plan a result of the initial 5 year plan update recently completed by the BTCSD. Proposed amendments to Chapter 1-3, 7-8, 10-11 of the Brooktrails Specific Plan provide clarification to existing text and addresses topics including on-street parking, community circulation (second & third access, U.S. 101 Bypass), fire protection, water supply, and development standards.

LOCATION: Brooktrails Township located northwest of the City of Willits.

SUPERVISORIAL DISTRICT: 3

GOVERNMENT CODE 65950 DATE: Not applicable to legislative acts

PROCESS: The process by which a specific plan is amended is described in California Government Code Section 65453 which states that “(a) A specific plan shall be prepared, adopted, and amended in the same manner as a general plan, except that a specific plan may be adopted by resolution or by ordinance and may be amended as often as deemed necessary by the legislative body.” Staff has interpreted this section to indicate that an amendment of a specific plan, while completed in the same manner as a general plan amendment, does not count toward the total number of general plan amendments allowed per year.

PROJECT DESCRIPTION: The Brooktrails Township Community Services District (BTCSD) has completed a five-year review/update of the Brooktrails Specific Plan. The Specific Plan was first adopted in 1997. The BTCSD Board of Directors requests that the County Planning Commission and Board of Supervisors consider the proposed amendments to the Plan and accept the changes as presented by the District. The proposed amendments are associated with a variety of topics including alternate access route planning, financing of infrastructure/plan implementation, parking, site development/construction standards, and fire safety.

APPLICANT'S STATEMENT: See Exhibit #1 - Resolution No. 2004-1 from the Brooktrails Township Board of Directors.

BROOKTRAILS TOWNSHIP CSD PROPOSED AMENDMENTS: See Exhibit #2 for a complete list of changes as proposed by the BTCSD. See Exhibit # 3 which presents the proposed revisions within the context of the plan itself.

RELATED PROJECTS: #SPA 1-2000 added development standards (beyond those which are currently included in Chapter 10 of the Brooktrails Specific Plan) relative to the construction of single family dwellings within the Township.

REFERRAL AGENCY COMMENTS: Agencies were asked to comment on impacts associated with this project. Relevant comments are included below. (C= Comment; NC = No Comment).

AGENCY	AGENCY
Mend. Co. Dept. of Transportation: C	Caltrans: C
Environmental Health: NC	Willits City Planning NC
Soil Conservation Service: NC	Dept. of Forestry: NC
Willits Unified School District: NC	U.S. Fish and Wildlife Service: NC
Air Quality Management District: NC	County Water Agency: NC
Dept. of Fish & Game: NC	Building Inspection: NC
Emergency Services: NC	Regional Water Quality Control Board: NC
Department of Health Services: NC	

CALTRANS - State Department of Transportation (Jackman, March 7, 2003): The following issues were included in the response from Caltrans and are summarized below (see Exhibit #4 for complete response):

- Caltrans recommends that the Specific Plan specify a funding source for the proposed improvements to Sherwood Road.
- The Specific Plan should clarify the location and priority associated with any/all proposed additional access routes (second and third accesses).
- Caltrans is in the process of studying alternative bypass routes for US Highway 101 around the City of Willits. A bypass route has not been chosen at this time and may impact choices relative to a second/third access route for Brooktrails.

Mendocino County Department of Transportation (Ellinger, March 3, 2003): Proposed access routes described in the Brooktrails Township Specific Plan should reflect proposals included in the draft Brooktrails Second Access Study prepared by the Mendocino County Department of Transportation – August 2002.

DISCUSSION OF PROPOSED AMENDMENTS/PROCESS: In October of 2003, Planning and Building Services staff provided the BTCSD with comments and suggested modifications to revisions as originally proposed by the BTCSD. Subsequently, the BTCSD incorporated suggested modifications as proposed by staff into the proposed update of the Specific Plan as reflected in attached Exhibit #2.

Staff’s suggested modifications focused upon the following topics: enforcement, financing of roadway improvements, clarification of jurisdictional responsibilities, on-street parking, second access route, water supply, Lake Emily, setback requirements, parking standards, and driveway slopes.

In conclusion, staff is in support of the proposed revisions to the Brooktrails Specific Plan as the revisions address changes that have taken place in the last several years, and also clarify several issues.

ENVIRONMENTAL RECOMMENDATION: Staff prepared an Initial Study assessing the potential for negative environmental impacts associated with this proposal. No negative environmental impacts are anticipated in association with approval of the amendment, and therefore, a Negative Declaration is recommended for adoption.

GENERAL PLAN CONSISTENCY RECOMMENDATION: The Brooktrails Specific Plan, upon adoption in 1997, was found to be consistent with the County's General Plan. The Specific Plan was adopted in-part for the purposes of:

- ◆ preservation of the natural appearance of the Township
- ◆ protection of the physical environment
- ◆ provision of erosion control practices
- ◆ protection of private property
- ◆ provision of public safety
- ◆ mitigation of traffic hazards

Staff finds the proposed amendments as outlined in this report further the goals and policies of, and is consistent with, the Brooktrails Specific Plan and the County's General Plan.

PROJECT RECOMMENDATIONS: Staff recommends that the Planning Commission recommend to the Board of Supervisors approval of Brooktrails Specific Plan Amendment #SP 1-2003 amending sections of the Specific Plan as detailed within this report and attached Exhibit #2, finding:

1. **Environmental Findings:** That an initial study has been prepared and a Negative Declaration of environmental impacts is recommended for adoption finding that no significant environmental impacts are anticipated;
2. **General Plan Findings:** The project is consistent with the Mendocino County General Plan and the Brooktrails Specific Plan; and
3. **Department of Fish and Game Findings:** An Initial Study has been prepared evaluating the potential for adverse impacts and, when considering the record as a whole, there is no evidence in the record that the proposal will have potential for an adverse effect on wildlife resource or the habitat upon which wildlife depends. Therefore, the Commission rebuts the presumption set forth in subdivision (d) of Section 735.5.

DATE

GARY PEDRONI
Senior Planner

Attachments: Exhibit #1 - BTCSO Resolution 2004-1
Exhibit #2 - Proposed Specific Plan Revisions
Exhibit #3 - Specific Plan Text with Revisions
Exhibit #4 - State Department of Transportation comments

EXHIBIT 1

RESOLUTION NO. 2004-1
A RESOLUTION TRANSMITTING THE FIRST FIVE-YEAR UPDATE
TO THE BROOKTRAILS SPECIFIC PLAN, TO THE BOARD OF SUPERVISORS
OF MENDOCINO COUNTY WITH THE RECOMMENDATION THAT THE PROPOSED
AMENDMENTS BE MADE TO SAID SPECIFIC PLAN

WHEREAS, the Mendocino County Board of Supervisors did, by Resolution No. 97-219, adopt the Brooktrails Specific Plan; and,

WHEREAS, the Brooktrails Specific Plan recommended periodic review and updates, at five-year intervals; and,

WHEREAS, the Board of Directors of the Brooktrails Township Community Services District has completed its first five-year review of the Specific Plan; and,

WHEREAS, the County of Mendocino, Department of Planning and Building staff has reviewed the proposed amendments, and made suggestions which have been incorporated into the update.

NOW, THEREFORE, BE IT RESOLVED BY THE BOARD OF DIRECTORS OF THE BROOKTRAILS TOWNSHIP COMMUNITY SERVICES DISTRICT does recommend that the Mendocino County Board of Supervisors adopt the proposed amendments to the Brooktrails Specific Plan, attached hereto as Exhibit A.

INTRODUCED and ADOPTED this 13th day of January, 2004, at a regular meeting of the Board of Directors of the Brooktrails Township Community Services District by the following vote:

AYES: Directors: Ziady, Horriek, Skezas, Pohlson, Orth
NOES: Directors: None
ABSENT: Directors: None


CHARLES A. ORTH, President

ATTEST:


MICHAEL CHAPMAN, Secretary

**PROPOSED AMENDMENTS TO THE BROOKTRAILS SPECIFIC PLAN
FIRST FIVE-YEAR UPDATE 2003/2004**

EXHIBIT 2

<u>PAGE</u>	<u>LINE</u>	<u>CHANGE</u>
1-2	21	Add intent statement after “recommendations.” as follows: <i>“The Specific Plan is not intended to obligate the District, or its staff, to enforce laws outside of its geographic boundaries. Financing of roadway improvements for the Specific Plan area will require collaboration between the County of Mendocino and Brooktrails with consideration given to local and regional benefits of new roadways.”</i>
1-4	2	Strike “eventually, a second new” from the sentence; Replace with “an.”
1-4	4	Strike “Southern” and replace with “Second”
1-4	7	Strike “Southern” and replace with “Second”
1-5	20	Strike the statement <i>“to make recommendations regarding the provision of facilities and amenities for off-site property owners, including possible development of time-share units”</i>
2-6	8	Insert “County roads and” before “community services” and strike the word “roads” after “utilities”
2-6	14	Add the statement <i>“The passage of zoning amendments, such as a hillside zoning ordinance, is the responsibility of the County of Mendocino. BTCSD can develop and propose an ordinance for consideration by the County. Additionally, it should be made clear that the District would be responsible for implementation of the ordinance consistent with related items included in the mitigation monitoring program included as part of the EIR adopted for the Specific Plan.”</i>
3-5		Replace Table 3.1
7-2	22	Replace the paragraph which states <i>“On-street parking is not allowed in Brooktrails per Mendocino County regulations. This is because local streets in Brooktrails have paved widths of 24 to 30 feet, and parked vehicles on streets result in insufficient clearances for a vehicle to pass without crossing over road centerlines”</i> with <i>“On-street parking may not be allowed in Brooktrails in certain instances due to insufficient clearances which would not allow vehicles to pass without crossing over road center lines. Limits upon on-street parking are applied to specific sections of County-maintained roadways on a case by case basis per Mendocino County Code administered by the County Department of Transportation and/or the BTCSD Fire Department.”</i>
7-3		Replace Figure 7-1, Circulation system
7-4	5	Strike <i>“extending from State Highway 20 to the Township”</i> and replace with <i>“from Sherwood Road to the Highway 101 bypass.”</i>

**PROPOSED AMENDMENTS TO THE BROOKTRAILS SPECIFIC PLAN
FIRST FIVE-YEAR UPDATE 2003/2004**

EXHIBIT 2

- 7-4 23 Strike *“(assuming an interchange),”*
- 7-4 24 Strike *“However, given that the Bypass project is not currently programmed for funding, and given the length of time it would take to approve, fund, design and construct a Bypass, Brooktrails should proceed as if the Bypass will not be constructed within the next 15 to”*
- 7-4 24 Insert *“The Mendocino County Department of Transportation has prepared a draft Second Access Study analyzing alternative alignments for the proposed Brooktrails Second Access. The preferred alternative would connect Sherwood Road with US. 101 near the northern interchange planned in association with the Willits bypass. The second access alignment may be modified in the future depending upon factors including the selected highway bypass alignment, interchange locations and construction costs.”*
- 7-5 1 Strike *“20 years. Therefore, the Southern “Access Route represents the best opportunity to accommodate future growth.”*
- 7-5 3 Strike *“The new second Southern Access Route is shown on Figure 7-1, Circulation System.”*
- 7-5 4 Change *“Southern” to “Second”*
- 7-5 6 Change *“Southern” to “Second”*
- 7-5 7 Strike *“A third access route is also shown on Figure 7-1. Given the uncertainty of the location and timing of the U.S. 101 Bypass, the precise location of the third access route should be considered schematic at this time, subject to additional study in the future.”*
- 7-5 32 Strike *“the Bypass project has not been included by the County or Caltrans on the list of fundable projects for the near future. Should the project re-surface prior to approval of the Southern Access Route, re-consideration for the third access route for priority as a second access route should be performed, especially if it will link directly with a new U.S. 101 interchange.”* Replace with *“the Bypass project is in its final DEIS/DEIR preparation stage. The District prepared comments as required by this Specific Plan, which are now of public record as part of any final Willits 101 Bypass project.”*
- 7-13 31 Replace the sentence *“This station currently employs a full-time fire chief, a typist, a primarily volunteer fire marshall and approximately 24 volunteers.”* with *“This station currently employs a full-time fire chief, a part-time office assistant, and approximately 20 volunteers.”*

**PROPOSED AMENDMENTS TO THE BROOKTRAILS SPECIFIC PLAN
FIRST FIVE-YEAR UPDATE 2003/2004**

EXHIBIT 2

- 7-14 1 Strike *“three”* replace with *“two”*, add *“a wild land truck”* after *“a squad truck”*, strike *“the Chief’s vehicle”* replace with *“two duty vehicles”*
- 7-14 2 Strike *“Township staff provide an additional seven auxiliary firefighters.”*
- 7-14 3 Strike *“in the Springcreek Subdivision”* from the end of the sentence.
- 7-14 4 Strike *“has six volunteer firefighters and two engines”* replace with *“is currently for equipment housing only.”*
- 7-14 5 Strike *“the City of Willits”* and replace with *“CDF.”*
- 7-14 5 Strike *“Currently, 50 percent of the fires that the Brooktrails Fire Department responds to are located outside of its District. Response times can be up to 20 minutes depending upon road and weather conditions.”* Replace with *“Approximately 12 percent of Brooktrails Fire Department responses are mutual aid outside the District.”*
- 7-14 26 Strike *“As a result of the Oakland, California fire, Chief Thomen attended a new course entitled “Wild land Interface Fire Fighting Essentials for Chief Officers”, first conducted in October of 1992.”*
- 7-14 31 Insert *“or disaster”* after *“wildland fire”*
- 7-15 1 Add *“public education”* after *“prevention”*
- 7-15 10 Strike *“ambulance service”* and replace with *“medical response”*
- 7-15 20 Strike *“an ambulance, a replacement of the”* before *“Chief’s vehicle”* and replace with *“a”*; Strike *“new”* before *“water tender”*
- 7-16 29 Change *“Summer Lake”* to *“Summer Meadow”*
- 7-17 29 Add the sentence *“In August of 2000 the District adopted Ordinance No. 113 allowing graywater systems, in an effort to reduce the flow of wastewater to the treatment plant.”* to the end of the paragraph.
- 7-18 29 Add the paragraph *“Additionally Brooktrails will investigate the possibility of raising Lake Emily dam by 15 feet. This would approximately double the capacity of the reservoir, thereby providing water for an estimated additional 700 SFRs. Also, an assessment of the potential to develop groundwater within the District was conducted in 2003 which provided a list of sixteen potential drill sites (See footnote 1 on page 11.1-2.)”*

**PROPOSED AMENDMENTS TO THE BROOKTRAILS SPECIFIC PLAN
FIRST FIVE-YEAR UPDATE 2003/2004**

EXHIBIT 2

- 7-18 34 Add *“On February 28, 2003 the California Department of Health Services, Drinking Water Field Operations Division (DHS) issued a compliance order to BTCSD prohibiting further water service connections. A Water Conservation Plan was adopted by the Board of Directors, wells are being developed to supplement the existing source supply, and the EIR process has begun for the dredging of the Lake Emily reservoir.”*
- 7-22 16 Strike *“Mr. Paul Williams, General Manager”*
- 7-23 30 Change *“Summer Lake”* to *“Summer Meadow”*
- 7-23 30 Correct the number of miles of trails from 7.5 to 9.
- 7-24 14 Change *“Summer Lake”* to *“Summer Meadow”* in Recreation Policy FS-7.5-3
- 8-1 20 Strike *“Non-rated (and wood shake or shingle) roofs, Class “C” and Class “B” roofs are more prone to incineration from sparks and cinders than are roofs rated Class “A”. The Ordinance applies to new roof structures with an average pitch of 3 to 12 or more and when 50% or more of the total roof area of a building or structure existing at the time of adoption of the Ordinance is reroofed (does not apply to accessory structures such as sheds, and appurtenant structures such as awnings and patio covers).”* Replace with *“The District conducts an annual table top drill with representatives from governmental agencies responsible to evacuate Brooktrails’ citizens should a formidable natural disaster occur. Included in this program are the Brooktrails Fire and Utility Departments, Little Lake Fire District, CA Department of Forestry (CDF), Mendocino Emergency Services Authority (MESA), our local Neighborhood Emergency Services Team (N.E.S.T), the City of Willits Police and Administration Department, Mendocino County Sheriffs’ Department, CA Highway Patrol, Mendocino County of Planning & Building Services, and members of the Board of Directors.”*
- 8-2 9 Strike the sentence *“Since the plan was last revised in 1994, additional residents have located within the Township”* and replace it with *“The evacuation plan is reviewed each year at the annual table top drill.”*
- 8-2 31 Add the sentence *“All donated lots in the dam inundation area will be added to the Greenbelt.”* to the end of PUBLIC SAFETY POLICY PS-8.1C
- 8-3 2 Change *“Summer Lake”* to *“Summer Meadow”*
- 10-11 6 Change *“Summer Lake”* to *“Summer Meadow”*

**PROPOSED AMENDMENTS TO THE BROOKTRAILS SPECIFIC PLAN
FIRST FIVE-YEAR UPDATE 2003/2004**

EXHIBIT 2

- | | | |
|--------|--------|---|
| 10-16 | 15 | Strike “ <i>satellite dishes and other</i> ” from the sentence. |
| 10-29 | 3 | Delete item 4.4.3 “ <i>Swimming Pond: Provide public access to key swimming areas such as Summer Lake</i> ” |
| 10-30 | 33 | Change “5” to “4” (due to the renumbering of page 10-31) |
| 10-31 | 8 | Delete the word “ <i>Construction</i> ” at the beginning of the sentence. |
| 10-31 | 9 | Change “15’ of” to “12’ or”; Strike the words “ <i>street paving</i> ” and replace with “ <i>property line.</i> ” |
| 10-31 | 14 | Move item 4) “ <i>No recreational vehicles or boats shall be stored within front setbacks</i> ” to new section entitled “ <i>ENVIRONMENTAL AND AESTHETICS STANDARDS</i> ” to be added to page 10-47 as Section “G” (renumber remaining items) |
| 10-31 | 33 | Replace item 12.2) “ <i>Two bedroom dwelling units shall contain not less than 800 square feet of living area</i> ” with “ <i>Each unit of multiple family dwelling units containing two bedrooms shall contain not less than 800 square feet; or one bedroom and studio units shall contain not less than 600 square feet.</i> ” |
| 10-31a | 1 | Delete item 12.3) “ <i>One bedroom and studio dwelling units shall contain not less than 600 square feet of living area.</i> ” |
| 10-31a | 8 | Under “ <i>B. Parking Standards</i> ” add paragraph 2) “ <i>Development of off-street parking standards and a hillside ordinance are under review by the BTCSD Board of Directors.</i> ” |
| 10-31a | bottom | Add comment “[<i>Continued on page 10-35</i>]” |
| 10-32 | bottom | Add page number |
| 10-33 | bottom | Add page number |
| 10-34 | D | Change “30%” to “20%” |
| 10-34 | bottom | Add page number |
| 10-35 | 33 | In item 9) change “ <i>15 percent slope (1.5:10) or greater</i> ” to “ <i>slope of greater than 5 percent</i> ” |
| 10-35 | 36 | In item 10) change “ <i>16 percent (1.6:10)</i> ” to “ <i>14 percent (1.4:10) or greater</i> ” |
| 10-35 | 39 | In item 11) change “ <i>16 percent (1.6:10)</i> ” to “ <i>20 percent (2.10) or greater</i> ” |

**PROPOSED AMENDMENTS TO THE BROOKTRAILS SPECIFIC PLAN
FIRST FIVE-YEAR UPDATE 2003/2004**

EXHIBIT 2

- | | | |
|--------|-------------------|--|
| 10-35a | 4 | Add comment “[Continued on page 10-39]” |
| 10-36 | bottom | Change “ <i>Parking Standards</i> ” in footer to “ <i>Site Development Standards</i> ” |
| 10-36 | bottom | Add page number |
| 10-37 | A | Change “0 - 10% Paving required except for single-family dwellings” to “0 - 5% Paving required.” |
| 10-37 | B | Change “0 - 15%” to “5 - 20 %”; Strike “Pave all driveways.” Add “/carport” after “garage” |
| 10-37 | C | Change “15 - 20%” to “14 - 20 %” |
| 10-37 | D | Strike “20 - 25%: Engineer’s design of driveway and drainage required. Driveway turn-around required.” Insert “20 %+: Slope is not permitted.” |
| 10-37 | E | Delete “25%+: Slope is not permitted” |
| 10-37 | bottom | Change “ <i>Parking Standards</i> ” in footer to “ <i>Site Development Standards</i> ” |
| 10-37 | bottom | Add page number |
| 10-38 | top
diagram | Change “10% (1:10)” to “5% (1:20)” |
| 10-38 | top
diagram | Add “Garage or Carport” above “20' parking” |
| 10-38 | top
diagram | Change “16% (1.6:10) Maximum Slope” to “20% (2.10) Maximum Slope” |
| 10-38 | top
diagram | In statement below diagram, strike “only” and add “and carports” to end of sentence. |
| 10-38 | bottom
diagram | Change “9.51 (16%)” to “81 (14%)” |
| 10-38 | bottom | Change “ <i>Parking Standards</i> ” in footer to “ <i>Site Development Standards</i> ” |
| 10-38 | bottom | Add page number |
| 10-39 | Top | Add comment “[Continued from page 10-35a]” |
| 10-45 | 19 | Delete paragraph outline letters A, B and C under “ <i>Site Development Standards Relating to Fire Safety</i> ” |

**PROPOSED AMENDMENTS TO THE BROOKTRAILS SPECIFIC PLAN
FIRST FIVE-YEAR UPDATE 2003/2004**

EXHIBIT 2

10-45	36	Insert the word “ <i>not</i> ” after “ <i>2</i> ” <i>a/c on</i> ”
10-46	1	Add “ <i>structures</i> ” after “ <i>parking</i> ” and change “ <i>zero</i> ” to “ <i>12 foot</i> ”
10-47	1	Add “ <i>AND MULTIPLE</i> ” between “ <i>SINGLE</i> ” and “ <i>FAMILY</i> ” in title of Section F
10-47	17	In item 5) add “ <i>must be installed on all eaves and</i> ” after “ <i>Gutters and down spouts</i> ” before “ <i>painted to match adjacent surfaces</i> ”
10-47	18	In item 6) add “ <i>(including gutters)</i> ” to end of sentence
10-47		Add section “ <i>G. ENVIRONMENTAL AND AESTHETICS STANDARDS</i> ” “ <i>No recreational vehicles or boats shall be stored within front setbacks</i> ”
11.2-10	24	Change “ <i>6,188</i> ” to “ <i>6,068</i> ”; change “ <i>1,150</i> ” to “ <i>1,392</i> ”; change “ <i>5,038</i> ” to “ <i>4,676</i> ”
11.2-11	Table- line 2	Change “ <i>6,188</i> ” to “ <i>6,068</i> ”
11.2-11	Table- line 3	Change “ <i>1,150</i> ” to “ <i>1,392</i> ”
11.2-11	Table- line 4	Change “ <i>5,038</i> ” to “ <i>4,676</i> ”
11.2-11	Table- line 7	Change “ <i>1,150</i> ” to “ <i>1,392</i> ”
11.2-11	Table- line 8	Change “ <i>2,523</i> ” to “ <i>2,281</i> ”
11.2-11	Table- line 10	Change “ <i>5,038</i> ” to “ <i>4,676</i> ”
11.2-11	Table- line 11	Change “ <i>2,523</i> ” to “ <i>2,281</i> ”
11.2-11	Table- line 12	Change “ <i>2,515</i> ” to “ <i>2,395</i> ”
11.2-11	Table- line 13	Change “ <i>December 1995</i> ” to “ <i>June 30, 2002</i> ”

Chapter 1, Summary

The *Brooktrails Township Specific Plan* addresses the changes in the community and the priorities of its residents and other property owners. The Plan guides ongoing development of the 4,943-acre Brooktrails Proper subdivision, referred to herein as “Brooktrails Township”; its provisions do not apply to the adjacent Spring Creek and Sylvandale subdivisions, except with regard to existing service agreements.

The Plan sets forth a goal and policy framework and implementation programs for guiding ongoing development of this semi-rural residential community. The Plan assumes an ultimate buildout of 4,000 equivalent dwelling units and an ultimate population of 9,919 residents. The plan is based on the *Vision Statement* and the Goals and Objectives originally developed by the Community Vision Focus Group and their “pyramid groups”, as amended by the Township Board of Directors. It also draws upon extensive community input from a survey of property owners regarding their intentions to develop their property, two Community Congress meetings, a Bay Area Focus Group session, Specific Plan Advisory Committee meetings, and substantial oral and written correspondence presented to the Township Board of Directors. Paramount to this process and to the development of this Plan has been a commitment to seek to establish Common Ground as the community moves forward into the next Century.

As set forth by state law, the Specific Plan establishes the goals and policies to guide the location, intensity and character of land uses, the circulation pattern, necessary infrastructure, improvements, and implementation actions required to realize the Plan recommendations. The Plan also contains a financing program necessary to maintain community services and provide the infrastructure required to accommodate buildout of the Plan. *The Specific Plan is not intended to obligate the District, or its staff, to enforce laws outside of its jurisdiction geographic boundaries. Financing of the roadway improvements for the Specific Plan area will require collaboration between the County of Mendocino and Brooktrails for implementation by each agency in its specific jurisdiction of an effective infrastructure financing program with consideration given to local and regional benefits of new roadways.*

The major features of the *Brooktrails Township Specific Plan* are as follows:

- The overall land use objective is to promote environmental stewardship; this encompasses careful site design including measures to conserve water and energy;

- Circulation improvements which consist of two additional access roads to address existing access and emergency evacuation constraints--one linking the Township with State Route 20 to the south, and eventually, a second new *an* access road connecting the Township with U.S. Highway 101 to the east. At two lanes, the new ~~Southern~~ *Second Access Route* would begin to reach capacity as the Township begins to reach buildout at 4,000 units. The third access route would then be required to relieve traffic on Sherwood Road and the new ~~Southern~~ *Second Access Route*. Additional circulation improvements include but are not limited to:
 - ▶ Improvements at the Sherwood Road/Birch Street intersection.
 - ▶ Improvements to Sherwood Road including adding paved shoulders (four-foot minimum width), left-turn pockets between U.S. 101 and Poppy Drive, and a pathway along its entire length.
 - ▶ Extension of Ridge Road between Iris Drive and Goose Road.
 - ▶ Development of parking bays, as feasible, throughout the Township to address current narrow roadway constraints.
- A 1,600 acre-foot reservoir to meet domestic water supply requirements for a community of 4,000 equivalent dwelling units and a population of 9,919 individuals. Assuming development of 40 units per year, the reservoir would be needed in the year 2,023. This project will require subsequent environmental analysis prior to development.
- Community facility and infrastructure improvements that will serve the specific needs of the Township as it develops over time. These improvements include, but are not limited to:
 - ▶ Development of wells to supplement the existing water supply for up to 2,500 units.
 - ▶ A new raw water main connecting the new reservoir to the existing raw water main at Lake Emily.
 - ▶ Expansion of the existing water treatment plant at Brooktrails in its present location to accommodate growth to 4,000 units.
 - ▶ A new wastewater trunk main to replace or parallel the existing trunk main from the Township to the City of Willits Wastewater Treatment Plant to accommodate development beyond 2,000 units, and a second new line to accommodate 3,000 units.
 - ▶ A third fire station is anticipated in the near term in the vicinity of the airport to serve existing development. A fourth fire station and fire vehicles would be required to serve 2,000 units, and a fifth fire station and vehicles would be required to accommodate 4,000 units.
- Design Guidelines and Site Development Standards that: protect public health and safety; preserve and enhance environmental resources and public amenities;

and establish and enforce design standards that will protect visual quality, and the community's quality of life in the existing semi-rural setting.

- Under the Specific Plan financing program, four bond issues are recommended. The first bond issue will be needed in the near term to finance the second access road which is needed at this time. The second bond issue would be required to fund wells and preliminary reservoir work at about the year 2011. The third bond issue would be required for the new 1,600 acre-foot reservoir around the year 2023. The fourth bond issue would be required for the third access road, scheduled for construction at 3,500 SFRs around the year 2048.

Development impact fees are also anticipated. An impact fee schedule is estimated based on a proposed allocation of capital project funding between fees and bond assessments. Actual fee levels, however, will depend upon the actual timing of the projects and the authorization for assessments.

- The Plan assumes the continued involvement of an active citizenry to work with the Board of Directors in implementing the Specific Plan:
 - ▶ to continuously collect and update planning and environmental data;
 - ▶ to prepare annually both a Planning Report and a "State of the Environment" Report for the Township Board of Directors;
 - ▶ to assess the needs of the community's senior citizens;
 - ▶ ~~to make recommendations regarding the provision of facilities and amenities for off-site property owners, including possible development of time share units.~~
 - ▶ to make recommendations regarding new provisions or refinements for Specific Plan updates.

The Plan's success depends on the continuing community support to ensure that, once adopted, the plan is protected from unwarranted changes, and that its programs are implemented in accordance with the intent of its guiding goals and policies. In addition, the Plan's success depends on the combined efforts of all regulatory and service delivery agencies currently active in the Plan area, and, particularly, on the ongoing close cooperation between Brooktrails Township and the County of Mendocino.

Design, which addresses site development standards. Because Section Two of Chapter 10 establishes site development standards, this portion of the Specific Plan is "regulatory" and is adopted by ordinance and becomes part of the County Zoning Ordinance.

Because the Township is in an unincorporated area of Mendocino County, the Specific Plan will provide a bridge between the County's General Plan and future development within the Township in a more specific manner than is possible under the current Development Review procedures and community-wide zoning ordinance. The Specific Plan will also have implications for the provision of, and budgeting for *County roads and* community services, inclusive of utilities, ~~roads~~, fire protection and other functions and activities.

As further example, a Specific Plan can, as implemented, amend zoning ordinances to create more appropriate land use or density designations; address specific area concerns, such as hillside development; and can serve as a basis for other community planning policy such as the conservation of natural resources potentially affecting quality of life issues in a single package tailored to particular land uses in a specific geographic area. *The passage of zoning amendments, such as a hillside zoning ordinance, is the responsibility of the County of Mendocino. BTCSD can develop and propose an ordinance for consideration by the County. Additionally, it should be made clear that the District would be responsible for implementation of the ordinance consistent with related items included in the mitigation monitoring program included as part of the EIR adopted for the Specific Plan.* For the Township, the Specific plan process concludes with a new plan for Brooktrails and an Environmental Impact Report (EIR) for that plan. Under the California Environmental Quality Act, the Specific Plan is regarded as a "project," and allows for the formulation of specific mitigation measures to reduce environmental impacts which are factored into the Specific Plan for implementation.

The Brooktrails Township Specific Plan will serve as a comprehensive planning document for the Township as it continues to grow. It includes development goals and policies, land use regulations, a capital improvement program and an overview of financing measures. It integrates land use controls that replace the existing Development Review Board Ordinance and Interim Site Development Standards.

**TABLE 3.1
Township Characteristics**

1. Total number of existing residential lots (includes merged lots):	6,188	6,068 ¹
2. Total number of <u>improved</u> (built) residential lots (includes merged lots):	1,150	1,392 ²
3. Total number of unimproved (unbuilt) residential lots (includes merged lots):	5,038	4,676 ³
4. Average annual multiple residential lot ownership rate (buyer secures more than one lot):		11% ⁴
5. Number of residential lots in high constraint area:		985 ⁵
6. Number of <u>improved</u> (built) residential lots in high constraint area:		157 ⁶
7. Number of <u>unimproved</u> (unbuilt) residential lots in high constraint area:		828 ⁷
8. Average annual water consumption:		220 gpd ⁸ per SFR

Footnotes:

1) Derived as follows - 6,605 total lots minus the following:

Public Facility (Greenbelt)	371
Public Facility (Upper Nine)	3
Public Facility (Golf Course)	3
Public Facility (Community Center)	2
Public Facility (Fire)	2
Public Facility (Water)	20
Lodge	5
Timber Preserve	3
Forest Land	8
Reduction from lot merger	120
Total	447

Source: Brooktrails TCSD, Breakdown within the Brooktrails Township CSD, December 31, 1995.

Total residential lots (6,068) includes C-1 (Limited Commercial District) zoned lots for which single-family detached units are a permitted use (146 lots total).

2) Includes 13 improved lots (residential) on C-1 (Limited Commercial District) zoned parcels

3) Includes C-1 (Limited Commercial district) zoned lots for which single-family detached units are a permitted use

4) Confirmed by Paul Williams, former General Manager, through Planner Nancy Dankin, memo of 11/11/94
Also, Memo to Brooktrails TCSD Board of Directors from Paul A. Williams dated 08/25/94

5) Source, EIP Assoc. Lot count based on overlay with High Constraint Area Map, October 12, 1994, supplemented with additional field inspections in November 1994, with revisions made to the High Constraint Area Map in December 1994, based on the additional field inspections. Lot count includes Commercial zoning designations because residential land uses are permitted under the C-1 (Limited Commercial District) zoning designation.

6) Includes five developed lots in the C-1 District

7) Includes lots in the C-1 District because residential is a permitted use

8) Source: Brooks and Vogel, *Management Plan Update for Brooktrails Township Community Services District* November 1993, page 3-1.

7.1 TRANSPORTATION AND CIRCULATION

One of the biggest challenges facing Brooktrails is the management of traffic on local streets, which is projected to increase up to four times current volumes, without changing the rural to semi-rural character of the community. The other challenges include providing emergency evacuation options for residents, maintaining and expanding the network of trails, and coordinating growth and required circulation improvements with Willits and Mendocino County.

The Township street network was constructed at the time the initial subdivision was created with the result that some streets (Sherwood Road) do not meet current Mendocino County code requirements for street width or shoulders. The internal street system, comprised primarily of two-lane roads, conforms to the rugged topography of the area, and consists of nine local streets that connect to Sherwood Road, and winding residential streets that feed from various low density neighborhood areas.

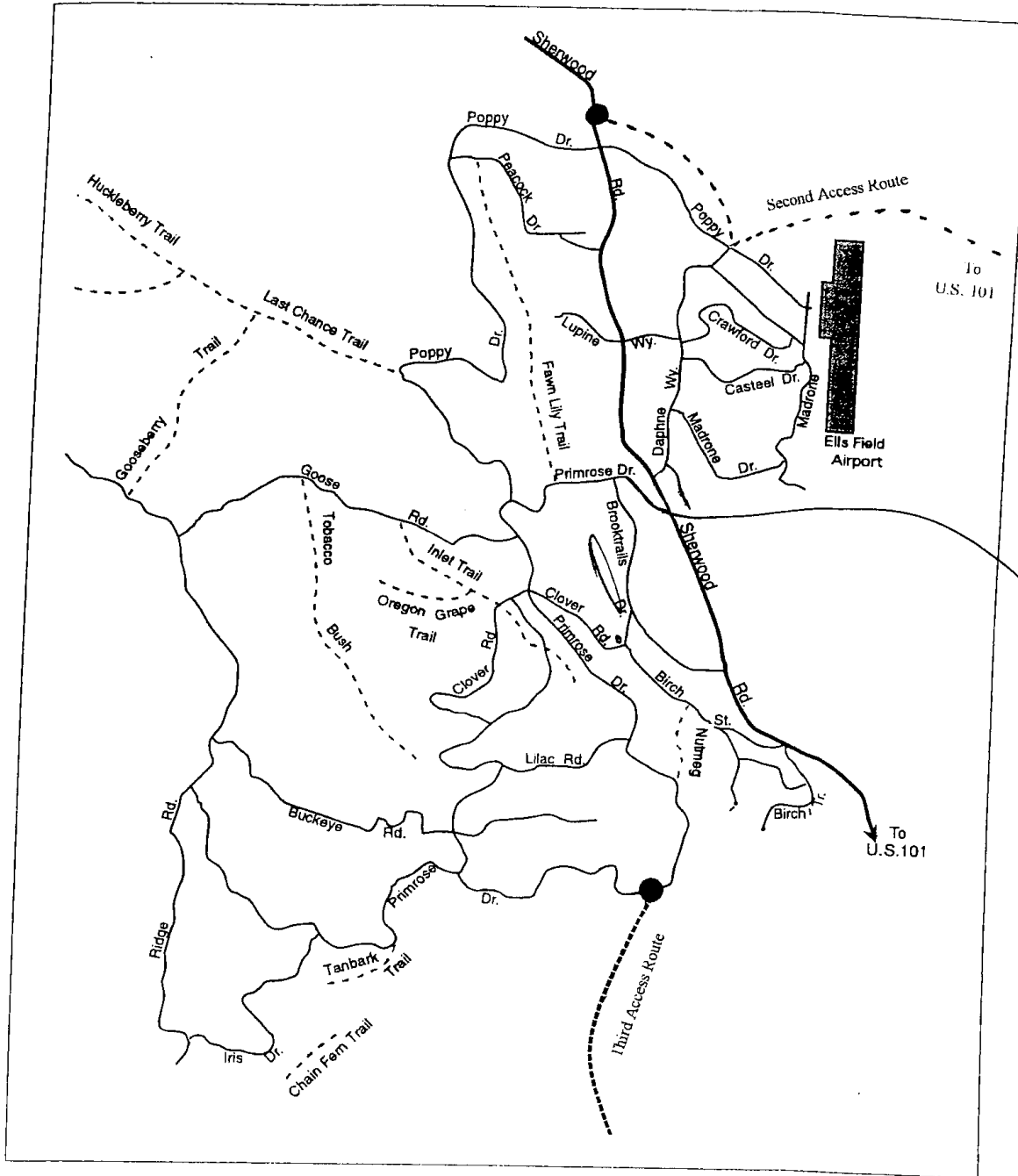
Sherwood Road provides the only connection from Brooktrails to U.S. 101 and the town of Willits. A two-lane county maintained facility, Sherwood Road meets U.S. 101 (Main Street), at a signalized intersection on the northern edge of the City of Willits. Aside from being the only access route into Brooktrails, Sherwood Road is characterized by its winding alignment, relatively steep gradients, tight geometries, lack of shoulders and passing lanes, lack of left turn pockets, and poor sight distances at several intersections. Unlike the other (newer) streets in Brooktrails, Sherwood Road evolved out of an old wagon road and is currently a typical rural facility. Consequently, Sherwood Road is the major constraint to future growth and already operates over capacity for a rural facility in the PM peak hour.

~~On-street parking is not allowed in Brooktrails per Mendocino County regulations. This is because local streets in Brooktrails have paved widths of 24 to 30 feet, and parked vehicles on streets result in insufficient clearances for a vehicle to pass without crossing over road centerlines.~~

On-street parking may not be allowed in Brooktrails in certain instances due to insufficient clearances which would not allow vehicles to pass without crossing over road center lines. Limits upon on-street parking are applied to specific sections of County-maintained roadways on a case by case basis per Mendocino County Code administered by the County Department of Transportation and/or the BTCSD Fire Department.

The roadway network within Brooktrails Township would not change substantially with additional development, with the exception of intersection improvements, paving portions of unimproved roadways, shoulder improvements, and two new access roads for public safety and to accommodate additional traffic that would be associated with Township growth.

The ultimate roadway system in Brooktrails at buildout would consist of a system of residential, collector, and arterial streets, plus new external connections outside the Township. The future roadway classification system is shown below in Table 7-1 and on Figure 7-1, Circulation System.



N
Not to Scale

BROOKTRAILS TOWNSHIP SPECIFIC PLAN
Figure 7-1
Circulation System

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Goals and Implementation Policies

TRANSPORTATION AND CIRCULATION GOAL FS-7.1-1: Improve vehicular access/egress to/from the Township and ensure adequate circulation within the Township.

TRANSPORTATION AND CIRCULATION POLICY FS-7.1-1A

Construct a new second Brooktrails Township access road ~~extending from State Highway 20 to the Township~~ *from Sherwood Road to the Highway 101 bypass.*

A second access road into Brooktrails Township is a recognized need at this time (see Chapter 8, Public Safety, Public Safety Policy PS-8.2A). In a series of continuing traffic and transportation studies prepared for the Township, the following was noted:

" ... Route #3 is selected as the preferred second access route largely because it provides immediate relief to Sherwood Road., bypasses the most congested part of Willits, and would provide a good second evacuation route for residents. A new signalized intersection would be required at the junction with S.R. 20, and the S.R. 20/Main St. intersection would require phasing adjustments. The new southern access route would be approximately 1.4 miles long and be designed as a two-lane divided arterial with shoulders, left turn lanes, and possibly passing lanes all within a 100 foot wide right-of-way. Peak hour capacity would be 1,360 vehicles per hour at Level of Service C, or roughly twice the capacity of Sherwood Rd."

A second connection to Brooktrails was studied by Mendocino County in 1991, with four alternative alignments subject to consideration. It was concluded that a Southern Access Route from Brooktrails to State Route (S.R.) 20 was the preferred alternative unless a U.S. 101 Willits Bypass was constructed. Caltrans has previously proposed constructing a U.S. 101 Bypass to the City of Willits to address existing and future traffic congestion. Six Bypass alignments are being studied. With the U.S. 101 Bypass, a direct connection to U.S. 101 north of Willits (~~assuming an interchange~~), was the preferred alternative. ~~However, given that the Bypass project is not currently programmed for funding, and given the length of time it would take to approve, fund, design and construct a Bypass, Brooktrails should proceed as if the Bypass will not be constructed within the next 15 to~~

~~*A Project Study Report was prepared by the Mendocino County Department of Transportation in 2000 to compare four alternative alignments for the Brooktrails Second Access. The County's preferred alternative was a new roadway joining Sherwood Road to Poppy Drive near the Daphne Way intersection, and then following an existing dirt logging road around the airport and down to U.S. 101. This route would provide direct access to the Willits Bypass if an interchange were constructed north of the City of Willits near the truck scales.*~~

The Mendocino County Department of Transportation has prepared a draft Second Access Study analyzing alternative alignments for the proposed Brooktrails Second Access. The preferred alternative would connect Sherwood Road with US. 101 near the northern interchange planned in association with the Willits bypass. The second access alignment may be modified in the future depending upon factors including the selected highway bypass alignment, interchange locations and construction costs.

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20 years. Therefore, the Southern Access Route represents the best opportunity to accommodate future growth.

The new second Southern Access Route is shown on Figure 7-1, Circulation System. At two lanes, the new Southern Second Access Route would begin to reach capacity as the Township begins to reach buildout at 4,000 units. A third access route would then be required to relieve traffic on Sherwood Road and the new Southern Second Access Route.

A third access route is also shown on Figure 7-1. Given the uncertainty of the location and timing of the U.S. 101 Bypass, the precise location of the third access route should be considered schematic at this time, subject to additional study in the future.

Given the long-term nature of implementing a second access route (approximately four to six years), the Township should begin proceeding with implementation immediately. Some of the first steps are to (a) come to an agreement with the City of Willits and Mendocino County on the need for, location of, and timing of the access route, (b) have the route adopted as part of each jurisdiction's general plan process, (c) and work with Caltrans in the implementation, funding, and coordination with the U.S. 101 Bypass project. Environmental review will be required under the provisions of the California Environmental Quality Act.

Implementation

Year: Begin planning studies for a second access route in 1997. Third access route required at about 3,300 to 3,500 dwelling unit count.

Implementing Agency/Entity: Brooktrails Township in collaboration with the County of Mendocino, Department of Public Works.

At buildout, Brooktrails will be one of the largest communities in Mendocino County. Almost all traffic destined to or coming from Brooktrails will travel through Willits unless the U.S. 101 Bypass is constructed. While some shopping trips will be reduced by the presence of commercial land uses within the Township, virtually all other work related and discretionary trips will be via U.S. 101 and S.R. 20. Coordination of improvements between Brooktrails and Willits is critical. Without improvements in Willits and the County, Brooktrails will not be able to reasonably accommodate future growth.

The U.S. 101 Bypass project profoundly impacts the preferred second access route. At the current time, the Bypass project has not been included by the County or Caltrans on the list of fundable projects for the near future. Should the project re-surface prior to approval of the Southern Access Route, re-consideration for the third access route for priority as a second access route should be performed, especially if it will link directly with a new U.S. 101 interchange the Bypass project is in its final DEIS/DEIR preparation stage. The District prepared comments as required by this Specific Plan, which are now of public record as part of any final Willits 101 Bypass project.

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Refer to the discussion of Implementation under Transportation and Circulation Policy FS-7.1-1A regarding the U.S. 101 Bypass project. Coordination with Caltrans will require a system of dialogue between the Township and Caltrans. Resolution No. 1989 16 adopted by the Brooktrails Township Board of Directors on July 27, 1989, in part, authorized the General Manager to establish and maintain a system for communicating in writing to other public agencies, including the State of California and County of Mendocino, the expression of District concerns and concerns of residents regarding matters which are related to the purposes and powers of the District to provide municipal services, including adequate circulation facilities.

Implementation

- Year: Ongoing.
- Implementing Agency/Entity: Brooktrails Township.

7.2 PUBLIC SERVICES

Fire and Police Protection

Fire Protection: In Mendocino County, fire protection is provided by 21 fire districts and volunteer organizations, the California Department of Forestry and the U.S. Forest Service. Assistance with inspections and arson investigations is available from the Office of the State Fire Marshall.

The Township maintains a fire department to meet fire suppression needs for the Township and the two nearby subdivisions of Sylvandale and Spring Creek. Local districts, such as Brooktrails, are responsible for the protection of structures, rather than wildfires. By law, the California Department of Forestry (CDF) is responsible for fire suppression for all wild lands, even those that are privately owned; therefore, CDF is responsible for fire suppression located within the 2,500 acres of parkland located in the Brooktrails subdivision, and all other vegetation fires within the Township.

The Brooktrails Fire Department and the CDF cooperate in serving a mutual protection area which includes Brooktrails, Sylvandale and Spring Creek subdivisions and occasionally areas outside these developments.

All fire districts in Mendocino participate in a mutual protection agreement which provides for backup in the case of large fires. During the summer, both the Brooktrails Fire Department and the CDF respond to all fires. During the winter, the Brooktrails Fire Department responds to all fires because CDF has a lower response capability. CDF is manned only in the daytime during the winter.

The Brooktrails Fire Department maintains two stations. The main station is located at 24680 Birch Street in Brooktrails. ~~This station currently employs a full time fire chief, a typist, a primarily volunteer fire marshall and approximately 24 volunteers.~~ *This station currently employs a full-time fire chief, a part-time office assistant, and approximately 20 volunteers.* It also

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contains ~~three~~ *two* fire engines, a squad truck, *a wild land truck* and ~~the Chief's vehicle~~ *two duty vehicles*. ~~Township staff provide an additional seven auxiliary firefighters.~~ A second station is located at the intersection of Ridge Road and Blue Lake Road ~~in the Spring Creek Subdivision.~~ This station ~~has six volunteer firefighters and two engines~~ *is currently for equipment housing only*. The Township also has a contract with ~~the City of Willits CDF~~ to dispatch fire calls. ~~Currently, 50 percent of the fires that the Brooktrails Fire Department responds to are located outside of its District. Response times can be up to 20 minutes depending upon road and weather conditions. Approximately 12 percent of Brooktrails Fire Department responses are mutual aid outside the District.~~

Fire Prevention: The Township takes precautions to prevent firestorms from occurring. Firestorms are a result of the buildup of fuel mixed with the right atmospheric conditions and a fire source. The key is to reduce the buildup of fuel and minimize the number of fire sources within the Township.

During the period of 1983 through 1985, the California Division of Forestry cut several fuel breaks in the Brooktrails Greenbelt area (see Figure 4-1, Existing Zoning Map). Since the late 1980's the Brooktrails Fire Department has taken specific steps toward minimizing the Township's fire exposure. For example the Department began writing Hazard Abatement Notices on vacant parcels, and entered into a Prescribed Burning Agreement with the California Department of Forestry.

Other measures being investigated include developing additional access into the Township (second access route as called for in the Specific Plan), continuation of the present program of enforcement of hazard abatement as described further below, the use of State forces on a limited charge basis, and expanding the fuel reduction program to beyond the Township's borders.

Annually, the Brooktrails Fire Department conducts a 12 hour course on wildland fire fighting and safety for all Brooktrails fire fighters. Areas covered include safety, wildland safety gear, fire behavior, use of the fire shelter, instruction in fire hose and fitting handling, use of hand tools and related fire fighting activities. This training is followed up with drills throughout the spring and summer seasons. The Fire Department also trains extensively in the incident Command System (ICS) which is the Command System used by all progressive fire departments. ICS is the only system that can be used effectively during a large scale disaster. ~~As a result of the Oakland, California fire, Chief Thomen attended a new course entitled "Wild land Interface Fire Fighting Essentials for Chief Officers", first conducted in October of 1992.~~

The Brooktrails evacuation plan would be put into use during a major wildland fire *or disaster* in Brooktrails (see Chapter 8, Public Safety). New stations and equipment are being planned for by the Fire Department. Class "A" fire rated roofing is now required on all new structures within the Township. Managing the Greenbelt to improve fire safety will be part of the Redwood Park Management Plan update.

Police Protection: Law enforcement for Brooktrails Township is provided by the Mendocino County Sheriff's Department and the California Highway Patrol. The County Sheriff is responsible for the enforcement of state laws and county ordinances, crime

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prevention, *public education* and apprehension of criminals in the unincorporated areas of the county. The County Sheriff's Department operates sub-stations in Willits and Fort Bragg.

The Willits substation is located approximately 3 miles from Brooktrails Township, at 125 East Commercial Street. This station serves the area between Ridgewood Grade (south of Willits) to the Humboldt County line. Response times for the County Sheriff's sub-station at Willits are hampered by the size of the patrol area and the terrain. Response times can vary from 10 minutes to an hour depending on officer location and the shift.

Goals and Implementation Policies

FIRE AND POLICE PROTECTION GOAL FS-7.2-1: Support and maintain adequate fire protection, police protection, ~~ambulance service~~ *medical response*, and other community services.

The Brooktrails Fire Department current 5-year plan calls for the addition of equipment and personnel. A new fire station is planned to be located at the Ells Field airport. Although the station is not planned as a result of growth in Brooktrails, it is planned in conjunction with the City of Willits and nearby community of Little Lake to improve response times within the area. This station would not be manned full time.

A second station is planned for a location north of the Spring Creek subdivision, north of Brooktrails, outside of the Specific Plan area. This station is planned as a result of growth in the area. A third new station, also planned as a result of community growth, will be located in the south-central portion of the Specific Plan area at a site to be determined, and would ultimately be manned. Planned equipment includes ~~an ambulance, a replacement of the~~ a Chief's vehicle, a ~~new~~ water tender, three fire engines, utility vehicle and squad vehicle.

FIRE PROTECTION POLICY FS-7.2-1A

Establish brush reduction and fire abatement programs consistent with the existing ecosystem and accepted forestry management practices.

FIRE PROTECTION POLICY FS-7.2-1B

Evaluate whether on-going development affords suitable fire protection. Consider weather, fuel and slope in determining the level of fire risk.

The use of Class "A" fire resistant roofing on building structures is discussed in Chapter 8, Public Safety. It should also be noted that the Brooktrails Township Board of Directors adopted Ordinance No. 98 on August 8, 1995, which provides for a procedure for the abatement of conditions constituting a public nuisance on private property. Ordinance No. 98 specifically notes the abatement of hazards respecting fire and wildfire in the Township, inclusive of "... dry grass, stubble, brush, dry or dead shrub, dead tree or limbs or any vegetative material, dead or alive, which by manner of its size,

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manner of accumulation, manner of growth or location constitutes a fire hazard, or when dry, will in reasonable probability constitute a fire hazard."

Such hazards are either noted by residents and reported to the Township, or noted by the Brooktrails Fire Department. The Township Board of Directors then, by Resolution of Abatement, authorizes the Fire Chief or his designee, to give notice to the property owner to abate the hazard (nuisance). If the property owner does not comply, the Fire Department may remove the hazard with subsequent costs to be borne by the property owner.

Implementation

- Year: Ongoing.
- Implementing Agency/Entity: Brooktrails Fire Department.

Solid Waste

Waste Enterprises, Inc., a private hauler of solid waste located in Willits, provides curbside collection and disposal services including recycling for residences in Brooktrails. The company is licensed and permitted by the County of Mendocino as the only hauler in the area, though technically the permit is not an exclusive franchise.

SOLID WASTE GOAL FS-7.2-2: Facilitate efficient solid waste disposal to ensure a safe and sanitary community.

SOLID WASTE POLICY FS-7.2-2A

Promote recycling of consumer and business waste to reduce landfill requirements and lengthen service of existing landfills, and to meet mandatory waste stream reduction requirements established by State law.

The Community Services District is currently in the process of entering into a Solid Waste Franchise Agreement. In addition the Township Board of Directors on February 27, 1996 adopted Resolution No. 1996-12 which specified District policy for waste reduction and directed the General Manager to implement those policies, inclusive of the following:

- 1) Establish waste recycling bins at the following District facilities:

Office Complex
~~Summer Lake~~ *Summer Meadow*
 Lake Emily
 Lake Ada Rose

- 2) Require that future copiers acquired by the District have a two-sided printing option.

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- 3) Require that District Offices use recycled content copy paper when it is cost effective.
- 4) Provide for recycling bins at all future multiple-family and commercial units as a part of development.

Implementation

- Year: Ongoing.
- Implementing Agency/Entity: Brooktrails Township.

7.3 UTILITIES

Water Supply: The existing Township water supply comes from the Township's Lake Emily and Lake Ada Rose reservoirs; combined storage capacity is approximately 400 acre-feet (AF). Water is distributed to all residents within the Township. The Township also operates a water treatment plant that has the capacity to treat 1.2 million gallons per day (mgd) of water. The Township is responsible for providing sufficient water for fire flow demands at all times. Water mains are located under the existing streets in Brooktrails.

Wastewater: The Township provides wastewater collection for most of its residents as well. It should be noted that there are approximately 660 unsewered lots in Brooktrails, most located at higher elevations within the Township. Of this total, about 616 lots are unimproved and about 44 lots are improved and on septic systems. The number of lots located on slopes 30 percent or greater is 203. Most of these lots are undeveloped and would only be sewerred if determined appropriate by the Township. Any costs of sewer improvements would be assessed to individual benefit areas at the appropriate time.

Wastewater collected within Brooktrails Township is treated at the City of Willits Wastewater Treatment Plant. The plant is a secondary treatment facility. The Township's contractual agreement with the City allows the Brooktrails community to use up to 39 percent of the treatment plant's capacity. The Township has purchased a portion of the plant capacity and pursuant to contract, pays a portion of the annual operating cost for the plant. The Township is responsible for providing hookups and maintenance of the system within Brooktrails. The Township also maintains the collection and pumping system within Brooktrails and controls new connections. Brooktrails wastewater is channeled through a meter which measures usage. *In August of 2000 the District adopted Ordinance No. 113 allowing graywater systems, in an effort to reduce the flow of wastewater to the treatment plant.*

The Township is currently using about 55 percent of the Township's wastewater treatment allocation at the City of Willits Wastewater Treatment Plant. Given the existing contract with the City of Willits, the Township could allow for the development of about 1,000 additional units before reaching its dry-weather wastewater-treatment allotment. The City of Willits will be upgrading their wastewater treatment plant and it

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is expected that an agreement will be negotiated for additional treatment capacity allocations. Expansion plans were designed in order to meet the requirements of the Regional Water Quality Control Board (RWQCB) criteria which require that excess capacity be maintained for wastewater treatment plants.

Storm Drainage: A storm drainage system was constructed for Brooktrails at the time of development. It was accepted by the County of Mendocino for maintenance and is maintained by the County Department of Public Works.

Storm water currently flows from rolled curbs on existing streets into catchments and culverts which drain into Willits Creek and its tributaries, ultimately winding its way through the City of Willits. At the present time, the storm drainage system is adequate, though localized flooding conditions do occur during unusual storms.

Utilities Goals and Implementation Policies

UTILITIES GOAL FS-7.3-1: Support and maintain adequate water supply, sewage treatment and disposal, and storm drainage facilities to serve existing and future residents and businesses.

UTILITIES POLICY FS-7.3-1A

Provide adequate new water storage and upgrade existing treated water storage facilities consistent with growth in a fiscally responsible manner.

Water supply is an immediate constraint facing Brooktrails, with the existing water supply system reaching capacity at about 2,000 residential units. Water infrastructure consists of a new dam capable of storing 1,600 acre-feet of water, plus water treatment commensurate with the level of demand. A new raw water main will connect the new dam (and reservoir) to the existing raw water main at Lake Emily (see Figure 4-2). The existing water treatment plant at Brooktrails will be expanded in its present location to accommodate growth to 4,000 units.

Additionally Brooktrails will investigate the possibility of raising Lake Emily dam by 15 feet. This would approximately double the capacity of the reservoir, thereby providing water for an estimated additional 700 SFRs. Also, an assessment of the potential to develop groundwater wells within the District was conducted in 2003 which provided a list of sixteen potential drill sites (See footnote 1 on page 11.1-2.)

On February 28, 2003 the California Department of Health Services, Drinking Water Field Operations Division (DHS) issued a compliance order to BTCSD prohibiting further water service connections. A Water Conservation Plan was adopted by the Board of Directors, wells are being developed to supplement the existing source supply, and the EIR process has begun for the dredging of the Lake Emily reservoir.

Development beyond about 2,000 units would require the expansion of sewer capacity. A new trunk sewer main will be necessary to replace or parallel the existing trunk main from the Township to the City of Willits (see Figure 4-2).

For additional information regarding additional water supply and the timing of providing additional water supply, see Chapter 11, Capital Improvements and Financing, Section 11.1, Capital Improvements.

Implementation

- Year: Begin design of dam, water main and treatment plant at about 2,000 units. Begin design of the sewer main at about 1,500 units.
- Implementing Agency/Entity: Brooktrails Township.

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PUBLIC INVESTMENTS POLICY FS-7.4-1B

Charge appropriate fixed fees for Township fire protection, water service, sewer service and related capital improvements for developed and vacant parcels on an equitable basis.

The Township Board of Directors, on June 20, 1995, through Resolution No. 1995-22 approved a 5-year Capital Improvement Plan for the 1995 to 2000 period. The Capital Improvement Plan contains a list of proposed infrastructure projects (sewer and water projects), fire equipment acquisitions, maintenance items, professional services and other items, together with budgets for each fiscal year up to the year 2000. In addition, the Township Board of Directors, on June 20, 1995, through Resolution No. 1995-21 adopted the Appropriations Budget for the 1995-1996 fiscal year. Each year, the Township prepares a Budget for the upcoming fiscal year for review and adoption by the Board of Directors. This detailed document contains listings of projected revenues and expenditures for the programs and services the District provides and is available for inspection at the District Headquarters located at 24860 Birch Street, Willits, California 95490. ~~Mr. Paul Williams, General Manager.~~

Water and sewer rates and charges were last reviewed by the District in May of 1995 for the 1995 to 1996 Fiscal Year. All budgeted items are subject to the approval of the Brooktrails Township Board of Directors at public meetings.

Implementation

The scale of public improvements needed to serve the projected growth in Brooktrails will require an expanded financing program from what exists today. At present, the District's capital needs are implemented through the District's 5-year Capital Improvement Plan and are paid through charges for service, connection fees, and standby charges. The projects currently in the Capital Improvement Plan are also small enough that they can be financed on a "pay-as-you-go" basis without seeking outside sources of capital. Some capital improvements in the future can be expanded incrementally, such as water storage tanks, fire vehicles, or Brooktrails' contribution to the Willits Sewage Treatment Plant expansion. These would be financed on a pay-as-you-go basis from ongoing revenues principally from development impact fees. Initially, however, the District may choose to levy impact fees on the basis of 100 percent pay-as-you-go financing until voter authorization for the proposed bond assessments can be secured.

Several projects needed for growth however, exceed what can be financed on a pay-as you-go basis. The new 1,600 acre-foot reservoir and access routes will require several one-time expenditures, such that a financing plan oriented toward the demands of growth will require some measure of bond financing to spread the one-time costs over several years (refer to the Financing Plan in Chapter 11, Section 11.4 for additional information). These bonds are expected to be funded through assessments levied on new development, and in some cases, existing developed parcels in the Township. The first bond, to be issued by the County, will be needed in the near-term to finance the second access road. As engineering studies for the

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second access road are being completed, the County would conduct the following activities as the County prepares to issue assessment bonds, as follows:

- Complete the necessary engineering studies.
- Hire a financing team including underwriter and bond counsel.
- Determine cost allocation among property owners.
- Submit the assessment to a vote.
- Conduct assessment district formation proceedings according to authority under which the assessments are to be levied.

The Township will construct a financing plan to be implemented in stages, responding to the level of growth and other circumstances at such time as the capital improvements are needed. At each stage, the Township will make the appropriate adjustments to the facilities planned and financing arrangements in response to the needs at the time. New development will pay for its share of the facilities required for growth, with charges taking the form of both fees and annual debt service assessments.

For long-term improvements such as the proposed 1,600 acre-foot reservoir, the Township, at the direction of the Board of Directors, would commission refined cost estimates and engineering studies at the appropriate times in the future to keep the documentation of capital requirements current, and to allow timely funding and construction as the capital expansions are needed. Bonds for reservoir construction would be issued by the Township.

- Year: Ongoing.
- Implementing Agency/Entity: Brooktrails Township/Mendocino County.

7.5 RECREATION

The Brooktrails 2,500-acre Greenbelt area (Redwood Park), is a major open space and recreational resource for the community. This subject is discussed in Chapter 6, Environmental Resources, Environmental Stewardship Policy ER-6.1C. The Redwood Park Master Plan provides conceptual ideas for 14 neighborhood miniparks with facilities to be used by residents of each neighborhood; these facilities were never developed. The Township currently maintains a nine-hole golf course and children's recreation area, par course and picnic area at ~~Summer Lake~~ *Summer Meadow*. Approximately ~~7.5~~ 9 miles of hiking trails exist within the Greenbelt (see Figure 7-1).

Chapter 7, Community Facilities and Services

Goals and Implementation Policies

RECREATION GOAL FS-7.5: Enhance recreation opportunities and resources within Brooktrails Township.

RECREATION POLICY FS-7.5-1

Develop a Brooktrails Township Community Services District Recreation Plan and fund an associated budget to initiate and maintain recreational activities/facilities.

RECREATION POLICY FS-7.5-2

Encourage the development of recreational amenities consistent with the scale and character of existing development within the community in recognition of the desires of non-resident property owners to enjoy Brooktrails Township.

RECREATION POLICY FS-7.5-3

Plan and budget for the following: (a) par course maintenance; (b) trail system; (c) vista points; (d) community center; (e) ~~Summer Lake~~ *Summer Meadow* improvements; (f) Chapter 3 of the Redwood Park Master Plan.

Refer to the discussion above under Public Investments Goal FS-7.4-1 regarding budgeting and funding for community facilities, which includes recreation facilities. At the discretion of the Board of Directors, Community Services District Staff may be assigned to collect data and report on the recreation needs and desires of the Brooktrails Community. Reports to the Board would be prepared and filed on a schedule to be determined. Reports to the Board may contain an inventory of existing facilities and their condition, needed maintenance activities, new equipment and/or facilities needed for the community, possible funding sources for new equipment and/or facilities, recommended recreation programs, and user demand projections.

Currently, Chapter III of the Redwood Park Master Plan, entitled The Master Plan, is out of date and needs to be amended. Amending the Master Plan, Chapter III, would be coordinated through District staff for adoption by the Board, consistent with the overall recommendations of District staff regarding recreation facilities in the first (annual) report.

Recommendations regarding the community center (item d above), should also include the District Headquarters building. The building's condition and potential life span should be assessed and a replacement structure of adequate size and location convenient to the majority of Brooktrails residents considered in light of those findings. Sites for a new District Headquarters Building being considered by the District include the following:

- The southeast corner of Brooktrails Drive and Primrose Road. There is space for an office building and some parking;

8. PUBLIC SAFETY

8.1 INTRODUCTION

ABecause we have built many homes in a wildland setting, the possibility of a large scale disaster does exist in Brooktrails, Spring Creek and Sylvandale. A major disaster could result from a variety of causes, or a combination of events. The two types of disasters most likely to hit our area would be a wildland fire or an earthquake. An earthquake can occur at any time, however, during the summer months we could also be faced with a wildland fire as a result of the quake. Unfortunately, the probability of a wildland fire in our area large enough to call for evacuation is a good possibility.”

The above quote is the opening paragraph in the Brooktrails, Spring Creek, and Sylvandale Evacuation Plan. The Evacuation Plan provides a description of Township alarm (siren) systems, evacuation areas, primary and secondary evacuation routes, fire and medical support agencies and groups, and a series of maps in support of the text descriptions.

Fire hazard and the need for continuing a high level of fire protection is a recognized concern of Brooktrails residents. In continuing the fire protection effort, the Brooktrails Township Board of Directors on February 13, 1996, adopted Ordinance No. 100 requiring Class "A" roofs throughout Brooktrails, and authorizing a continuing education program regarding the use of fire retardant roofs. ~~Non-rated (and wood shake or shingle) roofs, Class "C" and Class "B" roofs are more prone to incineration from sparks and cinders than are roofs rated Class "A". The Ordinance applies to new roof structures with an average pitch of 3 to 12 or more and when 50% or more of the total roof area of a building or structure existing at the time of adoption of the Ordinance is reroofed (does not apply to accessory structures such as sheds, and appurtenant structures such as awnings and patio covers).~~

The District conducts an annual table top drill with representatives from governmental agencies responsible to evacuate Brooktrails' citizens should a formidable natural disaster occur. Included in this program are the Brooktrails Fire and Utility Departments, Little Lake Fire District, CA Department of Forestry (CDF), Mendocino Emergency Services Authority (MESA), the local Neighborhood Emergency Services Team (N.E.S.T), the City of Willits Police and Administration Department, Mendocino County Sheriffs Department, CA Highway Patrol, Mendocino County of Planning & Building Services, and members of the Board of Directors.

8.2 GOALS AND IMPLEMENTATION POLICIES

PUBLIC SAFETY GOAL PS-8.1: Provide for the reasonable protection of public health and safety of all Brooktrails residents and visitors.

PUBLIC SAFETY POLICY PS-8.1A

Update the community Evacuation Plan and increase community awareness respecting emergency response procedures.

Implementation

Updating the Evacuation Plan will be at the discretion of the Township Board of Directors. ~~Since the Plan was last revised in 1994, additional residents have located within the Township. The evacuation plan is reviewed each year at the annual table top drill.~~ A program of community awareness would include insuring all new or recent residents have a copy of the Plan and are aware of its contents. Education and dissemination of information should be emphasized. All residents should be encouraged to forward any questions they have about the Plan to the Township Board. Responding to questions would provide a format for updating subsequent versions of the Evacuation Plan.

- Year: Annually.
- Implementing Agency/Entity: Township Board of Directors.

PUBLIC SAFETY POLICY PS-8.1B

Locate future facilities related to emergency services or public safety in areas outside the dam inundation area, to the extent feasible.

The Lake Emily dam inundation zone is shown on Figure 3-1, Existing Conditions Map (see Chapter 3, Brooktrails Township Development, section 3.2, Township Characteristics). The existing Township offices, fire station and community meeting hall are located within the inundation zone. The discussion of new facilities related to emergency services and public safety are discussed in Specific Plan Chapter 7, Community Facilities and Services.

PUBLIC SAFETY POLICY PS-8.1C

Encourage compatible uses such as recreation facilities and other day-use facilities within the dam inundation zone. Minimize the development of additional dwellings within the dam inundation zone. *All donated lots in the dam inundation area will be added to the Greenbelt.*

The Brooktrails golf course is located within the Lake Emily dam inundation zone as is the Brooktrails children=s recreation area, par course and picnic area at ~~Summer Lake~~ *Summer Meadow*. There are also approximately 37 residential lots within the inundation zone, 9 of which are improved (have residences). The inundation zone was identified as a high constraint area to development in an Environmental Assessment (EA) prepared for the Township in 1993. Any unimproved parcels within the inundation zone to be acquired under a lot reduction program could be designated as open space within the Greenbelt (see Specific Plan Chapter 11) regarding lot reduction programs).

PUBLIC SAFETY POLICY PS-8.1D

Ensure adherence to Mendocino County Public Health and Safety Policies and associated implementation actions.

Public health and safety goals, policies and implementation programs are contained in the Mendocino County General Plan Safety Element. The Safety Element contains the general County goals related to safety, the policies to be followed in moving toward the general goals and specific standards and programs to be applied and carried out. Policies and programs address fire, flood and seismic hazards. Refer to Public Safety Policy 8.2C below for further information and implementation. See also Zoning Policy LU-4.1D in Chapter 4, Land Use and Planning, for a discussion of land use planning of Ells Field Airport and related safety issues.

PUBLIC SAFETY GOAL PS-8.2: Provide for improved community emergency egress and access.

PUBLIC SAFETY POLICY PS-8.2A

Provide an improved second access road to Brooktrails Township.

Previous traffic and circulation studies conclude that an additional access road into Brooktrails is warranted both from a level of service standpoint and from the need to provide an emergency access for public safety and evacuation. This fact was essentially corroborated in a report prepared for the District by transportation consultants Fehr & Peers Associates, Inc. memo to Paul Williams, Brooktrails Township, from Michael G. Jones, Fehr & Peers Associates, December 8, 1993, which stated in part on page 2:

A.... From an evacuation perspective, a second or third access route into Brooktrails is needed now. This is based on two factors. First, because of the predominate wind direction there is a high likelihood that a major fire would cut off Sherwood Road to the south, and also major sections of

[DRAWING - FIGURE 10.11]

Ridge lines

1.9.3 Major ridgelines and crests of hills should remain free of development; if developed, structures should be sited such that trees still provide a natural backdrop and silhouette so that development remains inconspicuous and does not dwarf natural features.

Community Amenities

1.9.4 Preserve views of key amenities such as ~~Summer Lake~~ *Summer Meadow* and Beeler Pond to highlight their importance to the community.

[PHOTO - FIGURE 10.11a]

2.1.17 Include site furnishings such as picnic tables, benches, drinking fountains, and refuse containers to make public areas more inviting.

[*PHOTO-FIGURE 10-16*]

2.2 Building Footprints

2.2.1 Design structures to integrate well with the natural topography and features of the site. Preserve and incorporate significant topography, streams, views, mature trees and tree groupings, and significant vegetation masses into the development process. Such features should be considered as strong site design determinants.

2.2.2 Develop structures outside of buffer zones designated for environmentally sensitive habitat areas or other resources, unless there is no other feasible site available on the parcel.

2.2.3 Maximize energy efficiency through appropriate siting/placement of structures.

2.3 Building Heights

2.3.1 Building heights shall be limited to 35 feet for all structures consistent with the provisions of the Site Development Standards.

2.4 Setbacks

2.4.1 Maintain minimum setbacks consistent with the Site Development Standards to minimize the visual impact of development. Also, avoid placement of ~~satellite dishes and other~~ fixtures within setback areas to minimize unattractive appearances from roadways and adjoining properties.

- 4.4.2 Picnic Areas and Play Areas: Maintain and refurbish picnic and play areas, as needed, so that they are inviting to residents and visitors.
- 4.4.3 ~~Swimming Pond: Provide public access to key swimming areas such as Summer Lake.~~
- 4.4.4 Golf Course: Maintain the golf course so that it remains a key amenity of the community.
- 5. Regulating Nuisances
 - 5.1 Dust, Fumes, Vapors, Gases and Odors
 - 5.1.1 Emission of dust, dirt, flyash, fumes, vapors or gases which could be injurious to human health, animals, or vegetation; detrimental to the enjoyment of adjoining or nearby properties; or which could soil or stain persons or property, at any point beyond the lot line of the commercial establishment creating that emission, shall be prohibited. In addition, no land use or establishment shall be permitted to produce harmful, offensive or bothersome odors, perceptible beyond their lot lines, at ground or habitable elevation. (Particularly applicable for any home occupations involving preparation of food products). The location and vertical height of all exhaust fans, vents, chimneys, or any other sources discharging or emitting smoke, fumes, gases, vapors, odors, scents or aromas shall be shown on any required plans.
 - 5.2 Light/Glare
 - 5.2.1 No land use or establishment shall be permitted to produce a strong, dazzling light or reflection of that light beyond its lot lines onto neighboring properties, so as to impair the vision of the driver of any vehicle in the subdivision.
 - 5.3 Noise
 - 5.3.1 Develop natural sound barriers such as landscaped setbacks and berms along major roadway corridors to minimize noise impacts. Avoid the use of sound walls.
 - 5.3.2 Excessive noise at unreasonable hours shall be required to be muffled so as not be objectionable due to intermittence, beat, frequency, shrillness or volume.
 - 5.3.3 The maximum permissible sound pressure level of any continuous, regular or frequent source of sound produced by any activity shall be established by the time period and type of land use district listed below.

Sound from any source shall not exceed the following limits at the property line of said source:

Chapter 10, Community Design

Sound Pressure Level Limits Measured in dB(A)'s:

	<u>7 a.m.- 10 p.m.</u>	<u>10 p.m. - 7 a.m.</u>
Commercial Districts	65	55
Residential Districts	55	45

1. Where the emitting and receiving premises are in different zones, the limits governing the stricter zone shall apply to any regulated noise entering that zone.
2. The levels specified may be exceeded by 10 dB (A) for a single period, no longer than 15 minutes in any one day.

5.4 Refuse Disposal

5.4.1 Non-hazardous refuse: Property owners and tenants shall dispose or provide for the disposal of all solid and liquid wastes on a timely basis and in an environmentally safe manner.

5.4.2 Hazardous: Locate all outdoor storage facilities for fuel, chemicals, or industrial wastes and potentially harmful raw materials, on impervious pavement, and enclose completely by an impervious dike high enough to contain the total volume of liquid kept in the storage area, plus the accumulated rainfall of a 50-year storm.

Storage tanks for "home heating oil" and diesel fuel, not exceeding 275 gallons in size, may be exempted from this requirement provided that there is no seasonal high water table within 4 feet of the surface, and that rapidly permeable sandy soils are not involved.

10.2 SITE DEVELOPMENT STANDARDS

These Site Development Standards are set in consonance with the Zoning Ordinance of Mendocino

- a. Preservation of the natural appearance of the Township.
- b. Protection of the physical environment.
- c. Provision of erosion control practices.
- d. Protection of private property.
- e. Provision of public safety.
- f. Mitigation of traffic hazards.

These standards are set forth with the understanding that Brooktrails Township is a community encompassing a number of differing environments and containing many parcels with unique conditions that require unique and creative design solutions. Therefore, the standards that follow are not to be interpreted as all-inclusive.

A. SITE DEVELOPMENT STANDARDS

Note: Unique site conditions on a parcel may be cause for a variance from standards 1 through 5 4, if adhering to those standards would create undue environmental damage and/or create unreasonable costs

and constraints on an otherwise appropriate construction project.

- 1) Front setback for all residential zones shall be not less than 20' deep from street frontage property lines, unless specifically permitted by the Township and by County Variance. Setbacks greater than 20' may be required to mitigate environmental or visual impact of a specific project.

Construction Front setback on lots with a slope of greater than 19 percent uphill or downhill from the street may be reduced to ~~15'~~ *12' or more* clear of ~~street paving,~~ **property line** with the required sideyard increasing 6" for every foot of encroachment into the 20' front setback.
- 2) Corner lots shall have not less than 20' setbacks on both street frontage and a "back yard" outdoor area of not less than 1,000 square feet, not less than 15' deep from property line. Corner lot back yards shall be adjacent to key lot back yards.
- 3) Irregularly shaped lots shall have a rear yard opposite the street frontage not less than 20' deep nor less than 1,000 square feet in area.
- ~~4) No recreational vehicles or boats shall be stored within front setbacks.~~
- 5) 4) No structures above grade other than wood decks less than 3' above grade and fences no more than 6' high, shall be built within 5' of any property line.
- 6) 5) Height limits as required by County zoning are measured vertically from natural or excavated finish grade to the high point of the structure directly above. Height of structures on ridges shall not exceed the height of adjacent land or vegetation unless appropriate landscape screens area included with construction.
- 7) 6) All residential buildings are limited to 35 five feet in height. This is interpreted to mean habitable space. Garages and storage space only may be located under a two-story home as long as height limits are not violated.
- 8) 7) Each single-family and duplex residential dwelling unit shall have an enclosed general storage area other than wardrobes and closets, of not less than 40 square feet with a headroom of not less than 5'-6".
- 9) 8) Exterior wall and roof materials and colors shall be selected to be harmonious with the local environment.
- 10) 9) Roof surfaces facing the street or downhill slopes shall be broken into more than one plane.
- 11) 10) All roofs shall be class 'A' fire-resistive assemblies.
- 12) 11) Single-family dwellings shall contain not less than 1,000 square feet of living area.

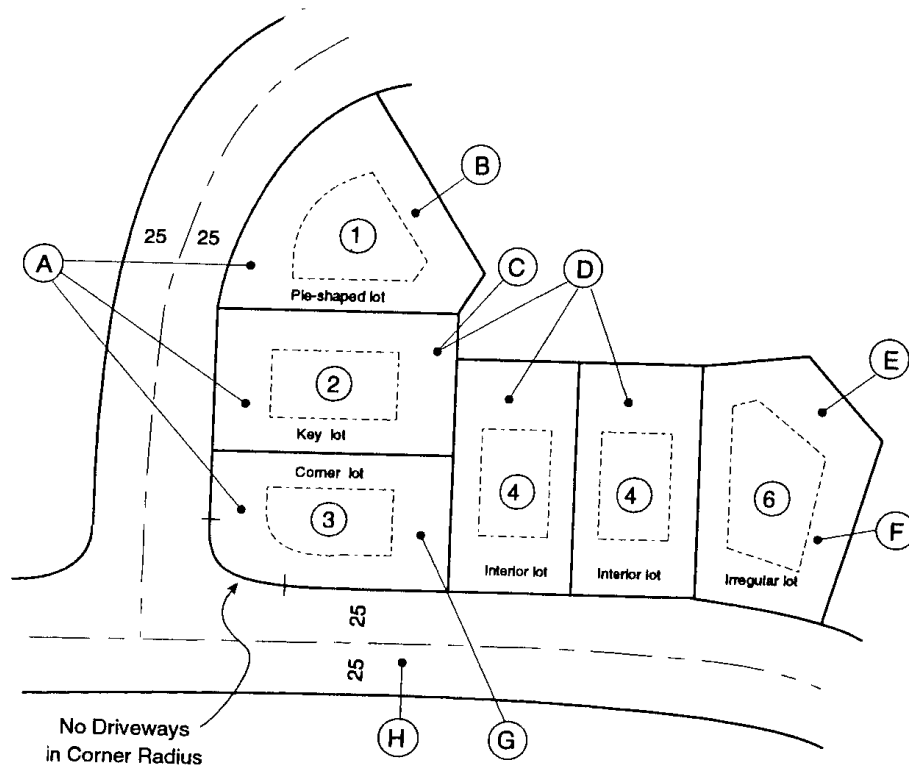
~~12.2) Two bedroom dwelling units shall contain not less than 800 square feet of living area.~~
- 12) *Each unit of multiple family dwelling units containing two bedrooms shall contain not less than 800 square feet; or one bedroom and studio units shall contain not less than 600 square feet.*

- ~~12.3) One bedroom and studio dwelling units shall contain not less than 600 square feet of living area.~~
- 13) On no lot shall any substantial development be permitted unless it includes a primary habitable structure of not less than 1,000 square feet.

B. PARKING STANDARDS

- 1) All driveways and parking spaces, except for single-family dwellings with driveway slopes 10% or less, are required to be paved with approved asphaltic or portland cement concrete surfacing over not less than 4" of compacted aggregate base. Parking spaces shall have a slope of not more than 5 percent (1:20).
- 2) *Development of off-street parking standards and a hillside ordinance are under review by the BTCSD Board of Directors.*

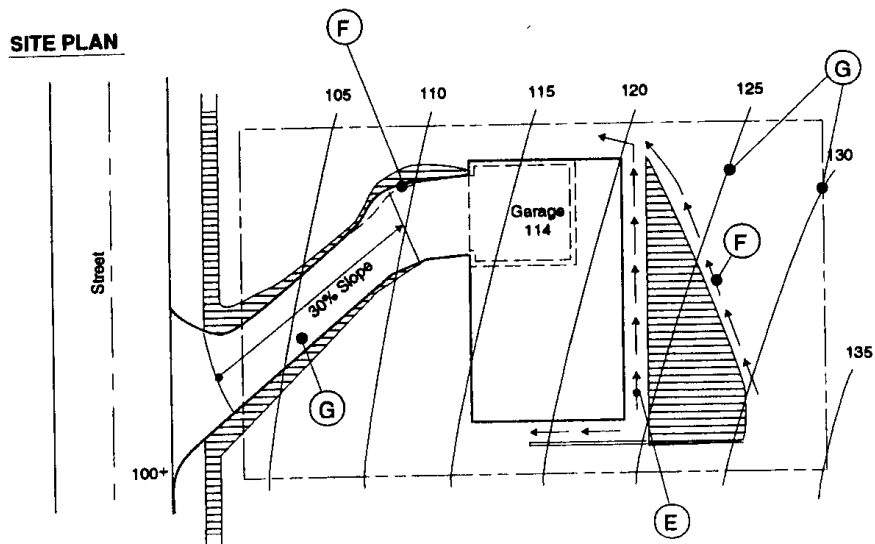
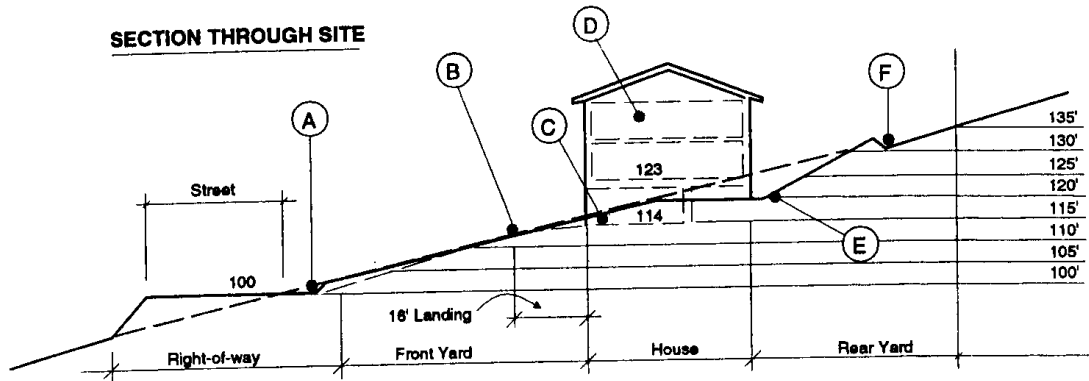
[Continued on page 10-35]



SETBACK STANDARDS

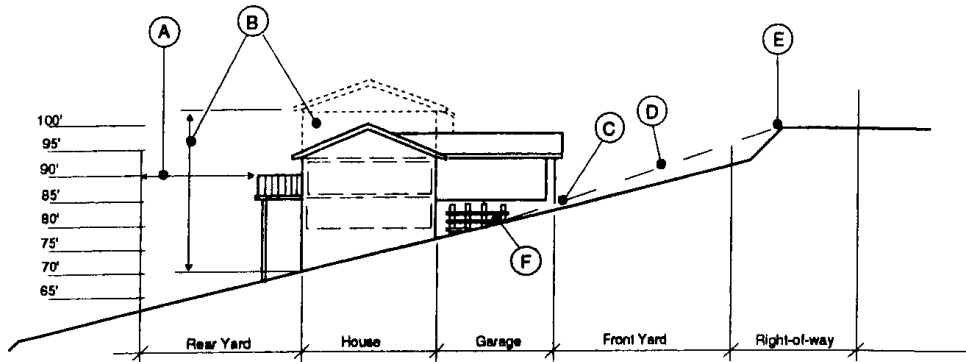
NOTE: Many lot configurations in Brooktrails are not illustrated here. Ask for assistance in resolving unusual conditions before starting project design

- (A) All yards fronting streets have 20' setbacks.
- (B) "Pie Shaped" lots shall have 1,000 square feet minimum rear yards & not less than 20' minimum
- (C) Key lot adjacent to corner lot has 20' rear setback.
- (D) Standard rear setback is 20' minimum.
- (E) Irregular lots need have only one rear yard if adjacent to green belt.
- (F) All side yards must have 6' minimum setback.
- (G) Corner lots shall have 15' minimum rear setbacks aligned with key lot rear yards.
- (H) In Brooktrails, cul-de-sacs have 40', streets 50', and collectors, 60' wide rights-of-way. (R.O.W.)

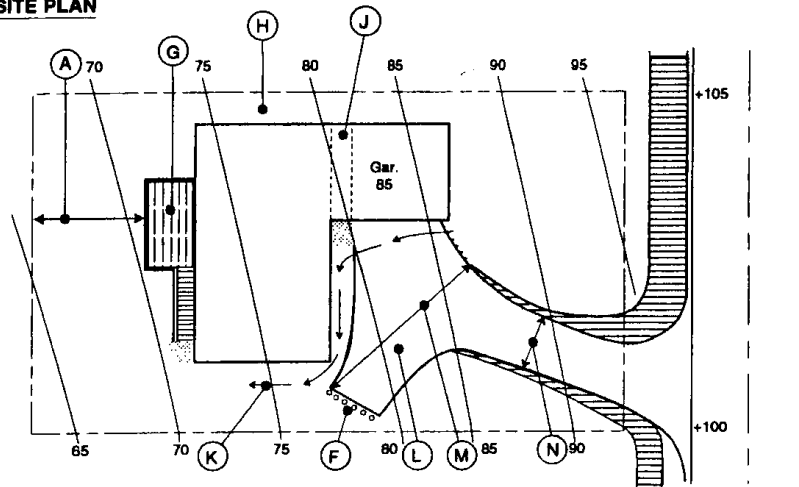


- A Bottom transition if slope exceeds 15% (1.5:10.)
- B Top transition if slope exceeds 15% (1.5:10.)
- C At 20% (2:10) slope, garage (parking) may have to be in a cut to avoid excessive driveway slope.
- D 2-story houses with smaller "footprints" for a given size, tend to minimize earthwork and EPM's required.
- E Provide drainage by surface run-off, French drain or pipe from behind building.
- F Berm & swale required at top of banks with positive run-off.
- G Driveway parallel with contours, 12' minimum wide.
- H 5' interval contours drawn (site has 25% slope).

SECTION THROUGH SITE



SITE PLAN



- | | |
|--|---|
| A 20' setback for decks 3' above grade | H 6' minimum side yard. |
| B 2 stories over parking or storage is very difficult on down-slopes | J Space in garage used for general storage. |
| C Bottom transition | K Show drainage pattern when diverted. |
| D 20% maximum slope | L Steep down-slope lot driveway often will Require turn-around (limited visibility at street) |
| E Top transition | M 40' minimum turn-around |
| F Retaining wall with visible posts. Behind turn-around | N 12' minimum width. |
| G Show decks, porches, fences, patios, etc. on Site Plan | |

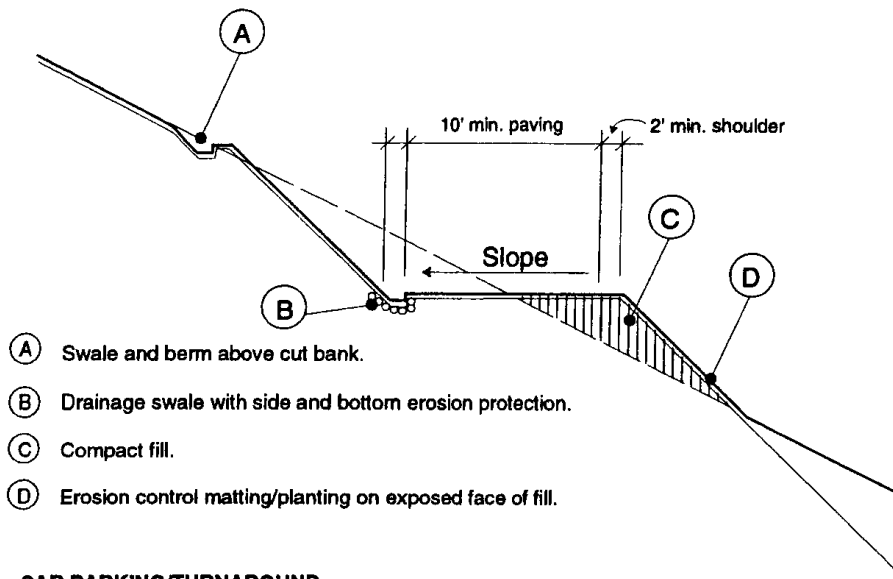
- Parking on lots of greater than 19 percent slope up or down from the street may be within the front setback, but not less than 15' from street paving.
- 2) Single-family residences shall have not fewer than two off-street parking spaces, with at least one space covered, located outside of the front setback, unless specifically permitted otherwise by the Township.
 - 3) Duplexes shall have not fewer than four paved off-street parking spaces, two of which may be located in the front setback if placed parallel to the street.
 - 4) Multiple family dwellings shall have not fewer than 1 paved off-street parking spaces per dwelling unit, four of which may be located in the front setback if placed parallel to the street and not less than 5' behind the front property line.
 - 5) Parking area dimensions shall be:
 - Open & Carport Parking:
 - 10' wide by 18' deep (one car).
 - 9' wide by 18' deep (more than one car).
 - Garage Parking:
 - 12' wide by 20' deep (one car).
 - 10' wide by 20' deep (more than one car).
 - 6) Only one driveway entrance per lot, not more than 24' wide, is allowed unless specifically permitted by the Township. Single family driveways shall be not less than 12', nor more than 18' wide. On merged lots, a maximum of two driveway cuts are allowed.
 - 7) Driveway entrances shall not be located within the radius of street intersections.
 - 8) Driveways opening onto Sherwood Road and collector streets must allow for turnarounds to permit both entering and exiting front first, except that if 200' visibility can be demonstrated both ways, backing out is permissible except on Sherwood Road. The following streets are designated collectors for the purpose of these Standards:
 - a. Primrose Drive from Sherwood Drive to Tulip Drive
 - b. Poppy Drive from Daphne Way to Primrose Drive
 - c. Daphne Way
 - d. Goose Road
 - e. Birch Street
 - f. Brooktrails Drive
 - 9) Driveways with a ~~15 percent slope (1.5:10)~~ or greater slope of greater than 5 percent, shall have a 16' deep level parking landing (5 percent maximum slope) in front of garages with doors and shall be wet weather surfaced.
 - 10) Driveways with a slope of ~~16 percent (1.6:10)~~ 14 percent or greater shall have an engineered profile drawn to scale showing grades, transitions and elevations at critical points, and shall have a turnaround to permit entrance and exit, front first.
 - 11) Driveways with any slope that exceeds ~~16~~ 20 percent (~~1.6:10~~) (2:10) are not permitted.
 - 12) Driveway fills must be compacted to 90% relative density.

C. ENVIRONMENT PROTECTION STANDARDS

Site Planning Criteria: The moving of earth, removal of natural vegetation, alteration of drainage patterns, and construction of roads, buildings, driveways and parking areas are all potential sources of

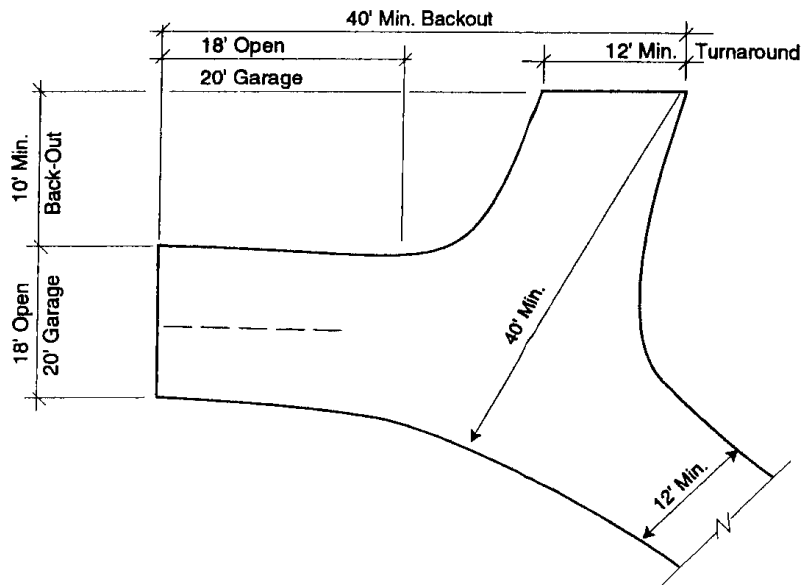
[Continued on page 10-39]

DRIVEWAY CROSS-SECTION

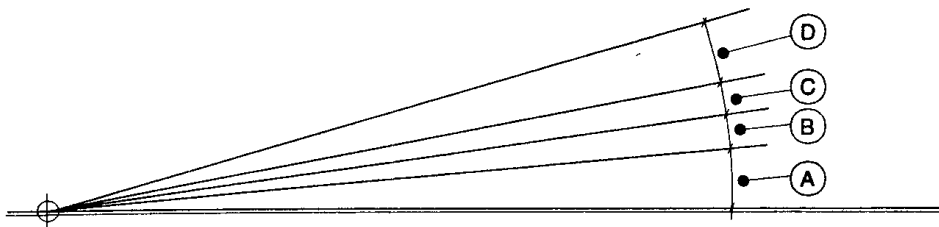


- (A) Swale and berm above cut bank.
- (B) Drainage swale with side and bottom erosion protection.
- (C) Compact fill.
- (D) Erosion control matting/planting on exposed face of fill.

CAR PARKING/TURNAROUND



BROOKTRAILS TOWNSHIP SPECIFIC PLAN
Parking Site Development Standards
Driveways & Turnarounds

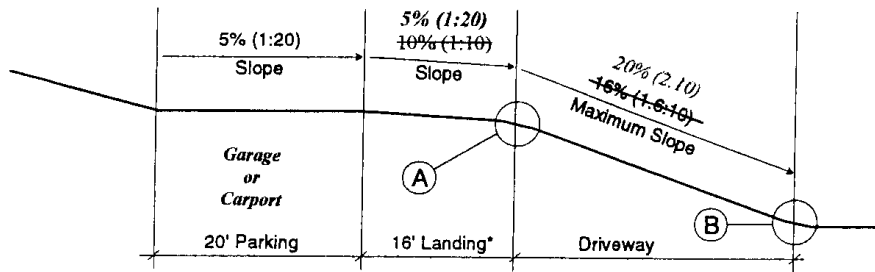


NOTES:

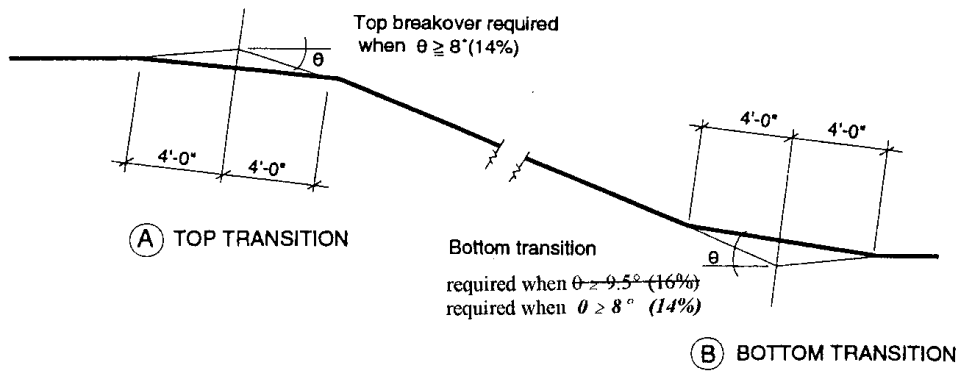
Slope is measured along center line of driveway (or center line of critical path of travel at two-lane driveways). *All driveways must be paved.*

- A 0 - 10%:
5% Paving required. ~~except for single-family dwellings.~~
- B 0 - 15%:
5 - 20% **Pave all driveways.**
Landing required in front of garage/*carport*.
Drainage design required.
- C 15 - 20%:
14 Transitions required at street and parking area.
- D 20 - 25%:
20%+ **Engineer's design of driveway and drainage required.**
~~Driveway turn-around required.~~
Slope is not permitted.
- E 25%+ **Slope is not permitted.**

DRIVEWAY SECTION (PROFILE)



* 16' Landing is **only** required in front of enclosed garages and carports.



BROOKTRAILS TOWNSHIP SPECIFIC PLAN
Parking Site Development Standards
 Driveway Profile

[Continued from page 10-35a]

Chapter 10, Community Design

damage to the environment that must be addressed and resolved during the planning process for a construction project in Brooktrails Township. In order to minimize environmental damage from a given project, the site planning process must 1) locate and identify salient physical features of the site, 2) design and locate project elements to fit the site, and 3) incorporate environment protection measures where necessary to protect the soil and vegetation and to minimize erosion.

All of the above elements must be incorporated on the project site plan, accompanied by structural details and a description of construction materials where necessary. The following list is a guide for specifics to be included on the site plan.

Site Plan Elements:

1. Physical Site Features
 - a. For all lots with a slope of 1:10 (10 percent) or greater, show 2' interval contours. For slopes greater than 2:10 (20 percent), use 5' interval contours.
 - b. Show property lines, paved roads, power poles and existing drainage collection devices and conduits on and adjacent to the site.
 - c. Show all trees with greater than 6" diameter, indicate size and type and identify all trees to be removed.
 - d. Show rock outcroppings and existing cut or fill areas on site.

2. Proposed Construction
 - a. Locate and dimension all structures, showing distances to property lines and finish elevation of the lowest floor (see Part A.).
 - b. Locate and dimension driveways, parking areas and turnarounds, including finish surface materials and slopes (see Part B.).
 - c. Show excavation, grading, cut and fill required by 2a and 2b, including:
 - 1) Finish grades of building/park areas.
 - 2) Height and slope of cut and fill banks (2:1 slope maximum unless otherwise designed by a professional engineer).
 - 3) Alterations to drainage patterns necessitated by 1) and 2) above.
 - d. Locate and describe proposed fences, decks, patios, fuel tanks, antennas, flagpoles, etc., including heights and finish material.

3. Environment Protection Measures (EPMs):
 - a. Fit the project to the site in order to minimize reshaping of the land and disturbance of the environment. After the physical features of the site have been identified, locate the proposed development in order to minimize land disturbance.
 - 1) Minimize earth movement. Fit development to the terrain. Minimize cuts and fills. "Balance" cut and fill wherever possible.
 - 2) Minimize impervious coverage. Make paved areas, such as, driveways and parking pads, as small as possible, consistent with other design requirements.
 - 3) Avoid steep slopes. Confine construction activities to the least critical parts of the site. Once these are disturbed by construction, the resulting erosion may be very difficult to stop. In addition, any construction activities on steep slopes will require the installation of costly EPMs.
 - 4) Align roads and driveways along slope contours. Locate driveways parallel to slope contours rather than up and down slope. Runoff down long or steep driveways tends to channelize flows and can cut deep gullies along the driveway.
 - 5) Retain the natural drainage system. Avoid confining any natural drainage system by placing it in a buried culvert or forcing it to a new location on site.

-
- c. Landscaping: is the best, long-term erosion protection method. Plantings are attractive, help retain earth and provide an environmental counterbalance to building construction.
- 1) Grasses should be planted as temporary erosion control (or permanent lawns) on all surfaces except steep slopes and poor soil or rocky cuts that will not support them. A number of erosion control seed mixes are commercially available for various environmental conditions and soil types.
 - 2) Perennial herbs, shrubs and vines are a superior long-term erosion control measure, once established. The following list of species is selected for superior erosion control characteristics. Some are natives, and many are drought-tolerant and deer-resistant. It should be noted that all plantings require watering until root systems take hold and deer will browse on many of them, but without serious damage to those identified as deer-resistant. Shade-loving species belong on exposed north to north-easterly slopes and under tree cover. All species listed should survive the coldest of Brooktrails winters, although some may require cutting back after a severe frost.

SITE DEVELOPMENT STANDARDS RELATING TO FIRE SAFETY

~~A.~~ ON-SITE SAFETY

1. Class "A" roof assemblies.
2. Brush and flammable waste clearance and control.
3. Chimney spark arresters.
4. Drywall all walls and ceilings, including garage, shops & storage rooms.
5. Visible address posting.
6. Open burning controls.

~~B.~~ SITE ACCESS (see Parking Standards included)

1. 24' minimum turning radius to centerline of driveway.
2. 16 percent maximum slope permitted. If permitted slope is reduced, hillside zoning standards (see D.) must be enabled in order to permit development of the majority of remaining undeveloped properties.
3. Pave driveways with slope over 10 percent.

~~C.~~ PUBLIC ROADWAYS

1. Paved section less than 32': parking one side only.
2. Paved section less than 24': no parking either side.
3. Property owner may widen paved section for the full length of property frontage to 16' from centerline (40' R.O.W.), 18' from centerline (50' R.O.W.) or 20' from centerline (60' R.O.W.) with 2" a/c on *not* less than 8" Class II aggregate based to gain parallel parking on public R.O.W., subject to approval of design and drainage by County D.P.W.

~~D.~~ HILLSIDE ZONING

(Applicable to R-1 developments only on properties demonstrated inaccessible using driveways of maximum permissible slope.)

1. Parking *structures* and storage structures may have ~~zero~~ 12 foot front setbacks, with 10' minimum sideyard tapering to 6' at a 20' setback (see diagram on page 10-17).
2. Parking openings onto roadway must total no more than 20' in width and be not less than 22' from paved roadway centerline for back-out visibility (20' @ 40' R.O.W.).
3. Provide 3' wide masonry or concrete stairs with handrails from street to house. Provide a 5' deep landing every 12' of rise.
4. Provide 2" schedule 80 galvanized steel water stand-pipe from street to hose cabinet accessible from stair landing 20'+ for house.
5. Uphill Property Standards:
 - a. Garage or carport height limit: 10' at front property line rising to 18' at 20' front setback, except that retaining structures may rise to the height of earth cuts.
 - b. House must be outside of front setback, except for entry and interior access stair adjacent to parking/storage structure. Entry must be set back 4' from property line.
 - c. 30' maximum width for parking structure and entry.
6. Downhill Property:
 - a. Parking structure height limit: 10' at front property line, 14' within front setback.
 - b. House must be outside of front setback.
 - c. Parking structure floor shall be at or near street level.
 - d. No floor of the house shall be higher than parking structure floor (excluding lofts within roof structures).
 - e. Parking structure floor must have asphalt or concrete surfacing. Floor structure and sub-structure shall be not less than 1-hour fire resistant.

E. CALIFORNIA DEPARTMENT OF FORESTRY REGULATIONS

Notwithstanding any of the above, the provisions of Public Resources Code 4290 and 4291 regarding basic wildland fire protection standards of the California Board of Forestry shall govern.

F. SINGLE AND MULTIPLE FAMILY DWELLING DESIGN STANDARDS

Because manufactured houses have pre-determined plan dimensions and are designed to be placed on level pads, many lots are not suitable for them. Variances for setbacks, tree removals and excessive excavation required to accommodate manufactured houses will not be permitted if they would not be necessary for homes designed to fit within prescribed setbacks and conform to the natural terrain.

The following guidelines are established in order to assure that single family dwellings, including manufactured houses, and associated accessory structures, meet the overall site development and design standards for residential development as set forth within this chapter.

1. Continuous perimeter skirting on concrete footings to match or be compatible with dwelling siding.
2. Entrance stairs or ramps and a minimum 20-square foot covered landing of materials and colors compatible with the dwelling. Rear entrance landing and decks need not be covered.
3. Roofs with a pitch of not less than 4:12, finished with a dark color to blend with Brooktrails= shadows and trees.
4. Roofs with a 22:12 minimum pitch over carports and decks finished to match dwelling.
5. Gutters and down spouts *must be installed on all eaves and painted to match adjacent surfaces.*
6. Eaves in open, grassy areas not less than 18@ deep (*including gutters.*)
7. Roof planes visible from the street or downhill slopes, broken into more than one plane.

Development plans submitted to the Brooktrails= Architectural Committee for review must include a detailed plot plan, grading plan and building plan which accurately represent the proposed development. Plan components are detailed in the building application materials supplied by the Brooktrails Township Community Services District.

G. ENVIRONMENTAL AND AESTHETICS STANDARDS

No recreational vehicles or boats shall be stored within front setbacks

Chapter 11, Capital Improvements and Financing

by other means, or lent at an appropriate rate. Various District resources could be used to fund any loan programs. If a charge identified for development reduction were applied to new development, these proceeds could be used as loans. Similarly, it is possible that balances in other Township funds could be used as well.

Greenbelt Adjustments

Given the amount of open space owned by the Township, it is possible that greenbelt adjustments could make land available to be used as incentives for property owners to retire development rights. For example, an owner of two or more marginally buildable lots might exchange these lots for a more desirable site on the edge of the greenbelt, resulting in a two-for-one swap and reducing development by one or more SFRs. Though such swaps would have to be reviewed on a case-by-case basis with respect to the locations of the retired and newly created lots and the integrity of the greenbelt, it is possible that greenbelt adjustments may permit owners to upgrade their development sites while at the same time reducing development potential.

Estimated Sources of Development Reduction

Naturally, the question arises as to the potential success of a program that relies solely on voluntary development reduction measures emphasizing mergers. To address this question an examination of the characteristics of different categories of lots in Brooktrails Township offers some indication of the potential success the District may expect. Beginning with a review of existing developed and undeveloped lots, the following discussion identifies sources and motivations for mergers, and estimates the approximate level of development reduction the District must achieve through the proposed program. Note that mergers are referenced in the discussions, though development reduction through conservation easements would apply as well.

Table 11.2-1 shows the existing developed and undeveloped lots in Brooktrails Township. Of the total 6,188 6,068 lots in the Township, 1,150 1,392 are developed, leaving 5,038 4,676 lots undeveloped.

Chapter 11, Capital Improvements and Financing

Maximum development in the Township has been measured according to water usage on the basis of equivalent single family residential units (SFRs). The Township has chosen to limit development to 4,000 SFRs, which translates into 3,673 actual lots. The difference between the number of lots and SFRs is explained by (1) the presence of multiple family lots which hold potential for more than one SFR of development, (2) 50 to 100 SFRs allocated to public uses, and (3) 35 SFRs reserved for Spring Creek. An additional 2,523 lots can be developed to reach the maximum of 3,673. The 2,515 lot difference between maximum development and the total number of lots in Brooktrails is the number of lots that would have to be merged or otherwise left undeveloped.

**Table 11.2-1
Development Potential and Development Reduction
Brooktrails Specific Plan**

		Lots ¹
TOTAL UNDEVELOPED LOTS		
Total Lots	6,188	6,068
Existing Developed Lots	1,150	<u>1,392</u>
Undeveloped Lots	5,038	4,676
DEVELOPMENT POTENTIAL		
Developed Lots Equaling 4,000 SFRs		3,673
Existing Developed Lots	1,150	<u>1,392</u>
Remaining Lots	2,523	2,281
DEVELOPMENT REDUCTION		
Undeveloped Lots	5,038	4,676
Remaining Lots	2,523	<u>2,281</u>
Difference	2,515	2,395
¹ Lot counts as of December 1995 June 30, 2002 Source: Brooktrails Township CSD; EIP Associates		

STATE OF CALIFORNIA BUSINESS, TRANSPORTATION AND HOUSING AGENCY

EXHIBIT 4

GRAY DAVIS, Governor

DEPARTMENT OF TRANSPORTATION

DISTRICT 1, P.O. BOX 3700
 EUREKA, CA 95502-3700
 TDDeaf Phone (707) 445-6463
 Phone: (707) 445-6412
 Fax: (707) 441-5869



March 7, 2003

1-MEN-101-47.24
 SP 1-2003

MAR 11 2003

Gary Pedroni
 Department of Planning and Building Services
 County of Mendocino
 501 Low Gap Road, Room 1440
 Ukiah, CA 95482

BY
 PLANNING & BUILDING SERVICES
 Ukiah, CA 95482

Dear Mr. Pedroni:

Thank you for giving Caltrans the opportunity to comment on the proposal to amend Brooktrails Specific Plan. The proposed amendments include changes to the "Transportation and Circulation" section of the plan. We have reviewed the proposal, and we offer the following comments:

- On page 1-4, proposed internal circulation improvements along Sherwood Road include: adding paved shoulders, left-turn pockets, and a pedestrian pathway. Caltrans supports the development of safe, appropriate facilities to accommodate both motorized and non-motorized travelers. Page 1-5 of the document indicates a financing program that includes four bond issues for specific projects. It also notes that development impact fees are anticipated. We recommend that the plan specify a funding source for the proposed improvements to Sherwood Road.
- The first paragraph on page 1-4 of the document discusses the development of two additional access routes for Brooktrails. It implies that the first priority would be to develop an access to Route 20—the second priority being the development of an additional access to Route 101. This is not consistent with Figure 7-1. Please clarify the priority of these two potential new access routes.
- Page 7-4 of the document refers to one of the potential new access routes noted in the previous comment. The uncredited quotation states, "... Route #3 is selected as the preferred second access route. ..." Please clarify if this "Route #3" would be the "second access" or the third access route, for consistency with figure 7-1.
- Page 7-4, second paragraph from the bottom, refers to the Willits Bypass Project. The second sentence should read: "Caltrans and FHWA circulated a Draft EIS/EIR in May of 2002, proposing construction of a new segment of U.S. 101 around Willits to improve safety and reduce delay; four bypass alternatives are being studied."
- The last paragraph on page 7-4 refers to a Project Study Report (PSR) on the development of a second access for Brooktrails. Please note that while a number of Draft PSRs have been provided to the public for review, no Final PSR has been signed and completed. The most recent Draft PSR is dated 8/13/02 and presents 7 alternatives.

EXHIBIT 4

Mr. Gary Pedroni, p.2

March 7, 2003

- Page 7-5 of the document, fourth paragraph, lists steps towards development of a second access route. We recommend that these steps include the following:
 - Finalize a PSR specifying scope, cost, and schedule for the proposed second access.
 - Work with the County and MCOG to secure funding for the project.
 - Include the second access in the Mendocino RTP.
- We did not receive a review copy of this document from the State Clearinghouse.

Caltrans supports and encourages efforts by communities to integrate land use and transportation decisions, projects, plans and activities. We support projects that increase non-motorized transportation and transit ridership, reduce vehicle miles traveled, address congestion, and foster a sense of community. We look forward to working with the County, the Township and the City of Willits achieve these goals.

If you have questions or need further assistance, please feel free to contact me at the number above, or Jesse Robertson of District 1 Community Planning at (707) 441-2009.

Sincerely,



Rex Jackman
Transportation Planner,
District 1 Community Planning

c. Phil Dow, MCOG